

Village of Palmetto Bay DUV Assessment

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WHY ASSESS THE DUV?

The Village of Palmetto Bay incorporated in 2002. In the years following incorporation, statutory requirements coupled with the community's desire to preserve the character of existing neighborhoods and define a unique identity for the heart of the community led to a series of public planning events and processes. The first public event was a charrette (2004). The citizen's vision resulting from the charrette - commonly referred to as the Franjo Triangle and Island District (FT&I) - was adopted in 2006. However, implementation of the vision did not occur as originally anticipated.

In 2013, in an effort to explore initiatives to spark development and create a vibrant mixed-use downtown core, the Village assembled the Downtown Redevelopment Task Force (DRTF). The DRTF was a team of local residents and professionals who met over a period of almost two years. They were tasked to build upon the FT&I and propose strategies and recommendations to achieve the Village's redevelopment goals. The work of this body was the basis for the creation and subsequent adoption of the Downtown Urban Village (DUV) land development regulations. The DUV was adopted on January of 2016 and replaced the FT&I.

The adopted DUV triggered development interest. It also raised concerns as some applications for development proposals appeared to be a significant departure from the scale and character depicted in the original citizens' vision and in conceptual renderings of the DUV.

The elevated level of community concern led the Village Council to establish a moratorium and hire a team of consultants (Dover Kohl & Partners and Marcela Cambor & Associates) to assess the DUV, consider community concerns and recommend ways to improve implementation of the downtown.

This report provides those recommendations and is organized as follows:

- **SECTION I: DUV REVIEW:** Comments, edits and suggested amendments.
- **SECTION II: WHAT WE HEARD:** Statements by community participants and some planning facts.
- **SECTION III: GENERAL OBSERVATIONS AND RECOMMENDATIONS**



Above: The DUV Sector Plan

SECTION I: DUV REVIEW

The consultant team reviewed the Village's DUV code. Included in this section is the entire DUV code. Notes and highlights have been included to identify opportunities and inconsistencies, suggest edits or amendments and provide general recommendations. This review and the information collected by the consultant team during multiple interviews and community meetings were the basis for the observations and recommendations included in Section III.

PB| downtown urban village regulations

A. Downtown Village (DV)

Sector Summary

DV
Downtown Village



NOTE 19A: This code doesn't appear to differentiate between the DV (Downtown Village) and DG (Downtown General). They appear to be different "in name only." The Village character is best with heights equal to those on the Island. Furthermore, this area is directly adjacent to single-family fabric. Bonus height should be limited to one additional story, provided that no portion of the development higher than 3 stories when adjacent to Urban Neighborhood boundaries.

Building Types	Lot Size W x D	Residential Density*	Building Height	Uses by Story	Private Open Space
Flexible Block	160'x160' (min)	24 du/acre	3 stories (min.) 5 stories (max.) 8 stories (with bonus)	1st 2nd+ C-R/O/R C-R/O/R Define streets where retail (and not O/R) is mandatory in 1st floor.	15% of site
Flex. Building	80'x100' (min)	24 du/acre	3 stories (min.) 5 stories (max.) 8 stories (with bonus) (see NOTE 19)	1st 2nd+ C-R/O/R C-R/O/R	15% of site

*Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve and/or TDR units.

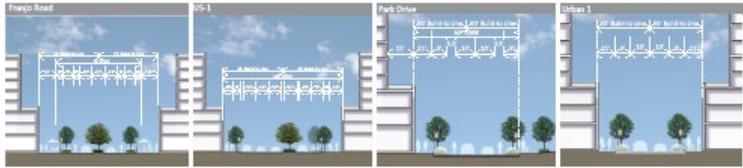
Streets and Building Placement

Street Type	ROW	Build-To Line Primary	Build-To Line Secondary	Sidewalk	Bike Lanes	Uses (at Street)	Glazing (at Street)
Frango Road (FR)	70'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	20'	Yes	C-R O R	70% (min)
US-1 (US1)	100'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	18'	N/A	C-R O R	70% min. (C-R/O only)
Park Drive (PD)	60'	30' (from centerline of road, up to 2 stories)	45' (from centerline of road, >2 stories)	10'	Yes	C-R O R	70% min. (C-R/O only)
Urban 1 (U1-U1)	50' or 60'	30' (from centerline of road, up to 2 stories)	45' (from centerline of road, >2 stories)	10'	N/A	C-R O R	70% min. (C-R/O only)

NOTE 19: The Downtown Village (DV) will set the character for the community. It is the area identified in the code as the center of the community, where residents and visitors alike will go to dine, shop, find entertainment, as well as live and work. However, the regulatory framework doesn't differentiate this area to be the most unique and special within the Village from the DG except in that it defines a lower maximum height. Minimums start to define differences relative to space, character or use. The Village needs to clearly define its objective for this area and the bonuses (if allowed) should be reflective of those goals.
Is the intent to develop a vibrant Village center? These bonuses (if allowed) should further this principle.
Is the intent to maintain a smaller-scale feel? Then the form should respond accordingly.
The regulatory language for this zone, like all others, should include appropriate transition to other zones, in particular those that are less dense/residential.

NOTE 19B: Why "just" if building height is allowed? Why regulate glazing independent of use? Glazing is better addressed at the building type level. Comment/questions apply to entire table.

Key: Commercial-Retail: C-R Office: O Residential: R



Village of Palmetto Bay Downtown Urban Village Regulations: 90-50.2B PB|18

Sample page with comments and highlights

30-50.23.1 GENERAL INTRODUCTION

Section 1.01 Purpose and Intent

This section provides the detailed regulations for the development within the Village of Palmetto Bay Downtown Urban Village (DUV), and outlines how these regulations will be used as a part of the Village's vision. The intention of this section is to facilitate development of a community village center within the Village of Palmetto Bay. This section will:

- provide for appropriate building and architectural scale through the inception of development standards that provide for a varied building form that responds to the individual districts within the Downtown Urban Village (DUV).
- promote and enhance commercial and civic street scene activity through adequate provisions for the inclusion of sufficient ground level retail commercial oriented uses and retail commercial architectural typologies;
- promote and enhance the architectural character of the Downtown Urban Village (DUV) through the inception of provisions that promote high-quality urban design form, architectural and Complete Streets design standards within the Downtown Urban Village (DUV).

Section 1.02 Application

The Palmetto Bay Downtown Urban Village Regulations (DUV) is guided by the these provisions herein, for the sole purpose of establishing form based regulations for development within the Downtown Urban Village (DUV). Where there appears to be a conflict between the DUV and other requirements of any other statute, law, or regulation, the most restrictive, and/or imposing the higher standard shall govern, unless otherwise noted.

Section 1.03 Organization

The DUV is organized into the following sections:

- **Regulating Plan and Uses**

Section 2 defines the sectors within the Downtown Urban Village (DUV) boundary, the parcels included within each sector, and describes, sector by sector the standards for building placement, design and use consistent with the permitted uses within the Village.

Section 2.07: Uses identifies the land use types allowed by the Village of Palmetto Bay in each of the sectors established in the Regulating Plans. Parcels within the Village of Palmetto Bay Downtown Urban Village (DUV) boundaries shall be designated only by land uses identified as permitted within the applicable sector.

- **Urban Design Standards**

Section 3 regulates the elements of development that affect the public realm. The urban design standards regulate building and parking placement, building height, and profile, and vary according to the location of the property

within a sector defined on the Sector Plan, Figure 2.

- **Architecture Standards**

Section 4 regulates the way in which development on each individual lot and block is developed to create an environment consistent with the intentions of the Village of Palmetto Bay Illustrative Vision Plan, through the implementation of two (2) main mechanisms: Building Types, Sec. 4.02-4.04 and Frontage Types, Sec. 4.05

- **Street Connectivity Standards**

Section 5 identifies conceptual location of new streets and guidelines for the design of new streets and the retrofit of old streets to support the intentions of the Village of Palmetto Bay Illustrative Vision Plan.

- **Definitions**

Section 6 identifies and defines the terms used in the DUV.

Section 1.04 Non-Conforming Uses and Structures

Nothing contained in this document shall be deemed or construed to prohibit the continuation of a legally established, non-conforming use or structure. The intent of this section is to encourage non conformities to be brought into compliance with these current regulations.

A. Nonconforming Uses

1. Legally established nonconforming uses may continue to operate so long as the use was legally established.
2. If a nonconforming use is discontinued for a period six (6) months, the use may not be reestablished. A use shall be considered discontinued once the activities or commerce, essential to the continuation of the use are abandoned. Discontinuance due to acts of force majeure shall not constitute abandonment provided that a good faith effort is made to reestablish said use.

B. Nonconforming Structures

1. Legally established nonconforming structures may continue to be used and maintained, so long as structure was legally conforming to the existing code at the time of construction.
2. Expansions, repairs, alterations and improvements to nonconforming structures shall be permitted in accordance with the following provisions:
 - (a) Internal and external repairs or improvements (general upkeep) that do not increase the square footage of the nonconforming structure shall be permitted and shall not be subject to the requirements of this section
 - (b) Expansions to a non-conforming structure shall be permitted as follows:
 - (i) If the total square footage of the proposed improvement, is less than or equal to 30% of the structure's square footage at the time

it became nonconforming. Any request for improvement shall require application for site plan approval. In addition, the property shall meet the tree requirements of 20 trees per acre of lot area or participate in the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Space Program's payment-in-lieu program for relief from standards of Sec.1.07 B

Note 2: Highlight seems to indicate that "bonuses" and considerations are not a "given" but subject to review by Mayor and Village Council.

- (ii) If the total square footage of the proposed improvement is greater than 30% of the structure's square footage at the time it became nonconforming, the entire structure and site improvements shall be brought into compliance with the DUV.
- 3. If a non-conforming structure is damaged by an act of force majeure, repairs shall be subject to the following provisions:
 - (a) If a repair/replacement cost is less than 50% of the building's assessed value, the structure may be reconstructed at the same height and within the same building footprint as permitted originally, provided a new application for building permit is filed within 12 months of the date of damage.
 - (b) If a repair/replacement cost is equal to or greater than 50% of the building's assessed value, the structure and site improvements shall be brought into full compliance with the DUV.

Section 1.05 Urban Design Review Procedure

All applications for development approval within the Village of Palmetto Bay Downtown Urban Village (DUV) shall comply with the requirements of section 30-30.5 and as more particularly required by this section and the review criteria in the sections to follow.

A. Application Process

All developments within the Village of Palmetto Bay Downtown Urban Village (DUV) shall be afforded the opportunity for site plan and architecture pre-application staff review. These informal meetings are to provide the applicant the opportunity to become familiar with the standards set forth in this code and to be advised on any site planning issues that may arise with regard to a development. Applicants are encouraged to present schematic plans of development with the idea being that all potential issues in the plan may be addressed before application submittal.

At any time, the applicant may submit an application for review by the Department, Figure 1. Applications for Site Plan Review shall be accompanied by exhibits prepared by a registered architect/landscape architect, submitted to the Department and shall include the following illustrations at a minimum:

- Application
- Survey

- Identification of development site area on all Regulating Plans;
- Identification of street cross-sections within the site plan and the primary frontage for the site;
- Site plans illustrating all proposed development on the site, including a table that indicates all area/frontage calculations, parking, landscape and open spaces that are required;
- All floor plans, elevations and sections of all buildings, for each floor and all dimensions/percentage requirements, including a corresponding table of building heights, square footage by use and number of residential units within the development;

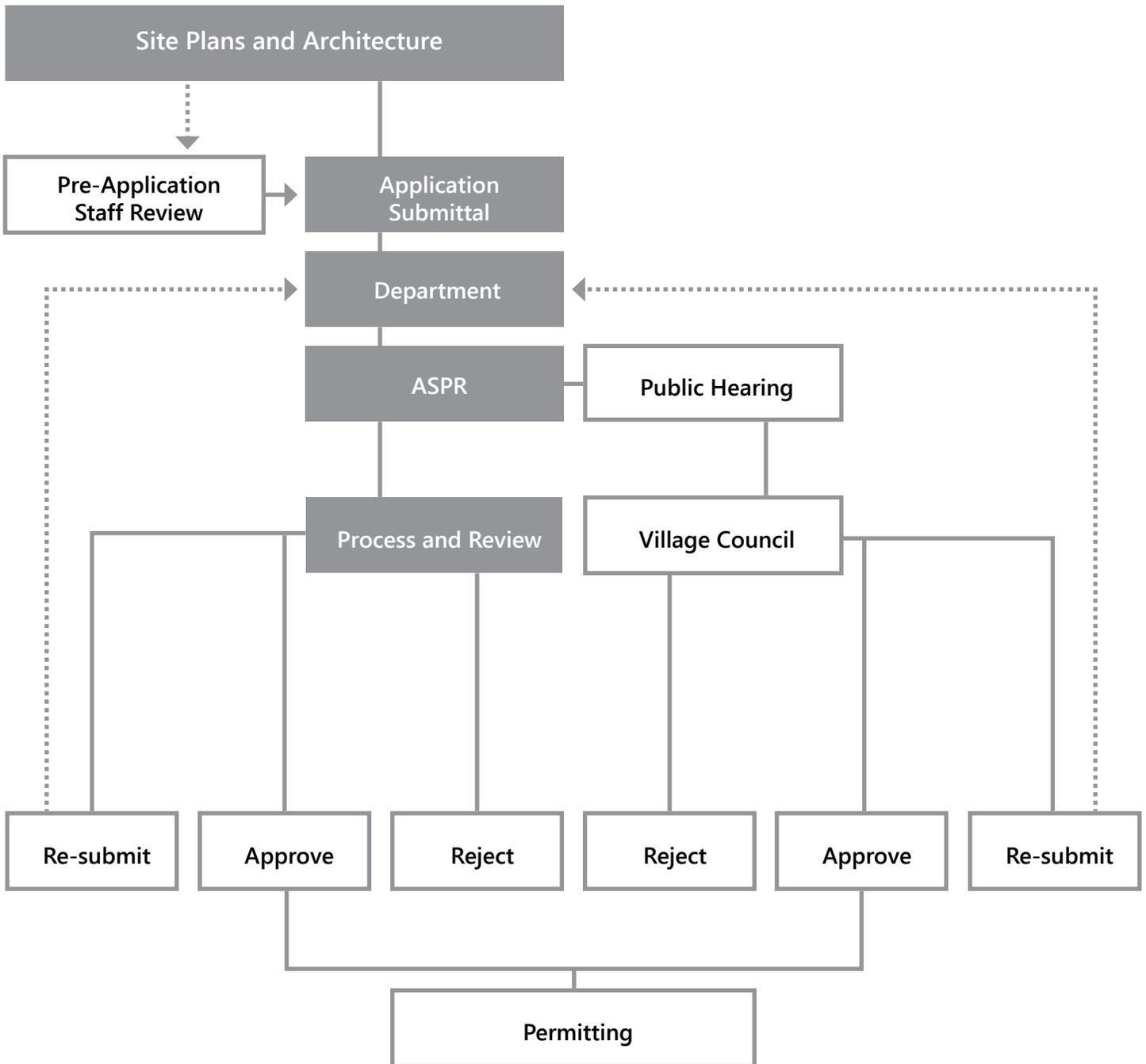
B. Site Plan Review Required

All applications for development pursuant to 30-50.23 shall require site plan review. Only those site plans that include variances, reserve residential units, reserve commercial floor area, transfer of development rights, development bonuses, any use which requires public hearing pursuant to the Village's Land Development regulations, and/or any other design considerations not contemplated by these provisions, shall be subject to public hearing review before the Mayor and Village Council.

C. Reserve Units and Transfer of Development Rights

1. Intent and purpose: The intent of this section is to provide incentives that encourage the development of the Downtown Urban Village (DUV). The goal is to incentivize development in a manner that is sustainable to achieve the stated Goal, Objective or Policies of the Village Comprehensive Plan, to facilitate appropriate redevelopment and revitalization, and to facilitate economic development. This section provides procedures to allocate reserve units and commercial floor area, as authorized and provided by the Comprehensive Plan, and the transfer of residential development rights (TDRs) between properties within the DUV.
2. Specific definitions: The words and phrases in this section shall have the meanings prescribed in this section, except as otherwise defined in Section 6.
3. Application: All requests for assignment of available reserve residential units, reserve commercial square footage, and/or transfer of development rights shall be by application as provided by the Village of Palmetto Bay. All property owners subject to the application request (including owner(s) of the sending site when the application includes a TDR) shall be party to the application. All reserve residential units, reserve commercial square footage, and/or transfer of development rights application request shall be filed with a site plan application of the receiving site pursuant to section 30-30.5
 - (a) All property owners subject to the application request (including the owner(s) of the sending site in the case of TDR) are encouraged to meet with staff prior to submission of an application. The

Figure 1



Note 4: The code states clearly that limitations may be imposed.

purpose of the meeting is to discuss the development and/or redevelopment of the property(s) and to understand any limitations that may be imposed thereupon.

- (b) All property owners subject to the application request shall be a signature to the application, and provide such information as requested by the Village to review the request. At a minimum, the information shall include the following:
 - (i) Identification of the affected properties;
 - (ii) Proof of ownership of the receiver site, and for TDR's proof of ownership of the development right(s) from the sender site;
 - (iii) A complete site plan application of the receiver site to be reviewed pursuant to Division 30-30.5.
- (c) The Village will review the application to determine:
 - (i) Compliance with the criteria of Sec.1.05 C 4
 - (ii) That the site plan application complies with the Land Development Regulations and Comprehensive Plan.

4. Village Council action and criteria for approval: After a public hearing, the Village Council shall adopt a written resolution granting, granting with conditions, or denying the reserve residential unit, reserve commercial square footage, and/or TDR request. To authorize any such request, the Village Council must determine that the following criteria have been met:

- (a) All property owners subject to the application shall be applicants to the request.
- (b) All properties subject to the application must be within the DUV zoning district.
- (c) For TDR's, the amount of residential units on the sender site cannot be reduced below 40% of the maximum base density permitted on the sender site property.
- (d) An application shall not be approved if the sender site has any active code violations.
- (e) The receiver site shall be evaluated for its viability as an area of increased development and shall be reviewed pursuant to Section 30-30.5, as reflective of the intended development.
- (f) All bonds, assessments, back City taxes, fees and liens (other than mortgages) affecting all properties subject to the application shall be paid in full prior to recordation of the warranty deed for the transfer of the development rights.
- (g) For TDR's, the validity of the sending site's residential development right(s) has been verified as available by the Director.

5. Allocation and Transfer Generally:

- (a) Prior to the issuance of a building permit authorizing the development of the receiver site, deeds of transfer, or other appropriate legal instrument, shall be recorded in the chain of title of all affected

properties containing a covenant prohibiting the further use of the development right(s) so allocated or transferred. Further, all impact fees and incentive based bonus fees must be paid in full.

- (b) Approved allocation of reserve residential units, reserve commercial square footage and/or TDR's shall expire if the time table for permitting/construction is not met pursuant Section 30-30.2(k) or as so provided in the approved phasing plan of the approved site plan application request. Requests for extension shall be filed pursuant to 30-30.2.
- (c) Unused reserve residential units and unused reserve commercial square footage that were not utilized within the required time period provided herein shall revert to the Village and return to the Village's reserve. In the case of TDR, residential units that were not utilized within the required time period provided herein shall be added to the Village residential reserve.

Section 1.06 Public Improvement Trust Funds

The Village of Palmetto Bay shall create the following Public Improvement Trust Funds:

A. The Village Centralized Parking Trust Fund

The Village Centralized Parking Trust Fund is created to collect funds to build parking facilities within the DUV. The fee schedule of which shall be established by an ordinance.

B. The Village Landscape and Open Spaces Improvement Trust Fund

The Village Landscape and Open Spaces Improvement Trust Fund is created to collect funds for the Village of Palmetto Bay to improve the quality, quantity and character of the right-of-ways, landscaping and public open spaces within the DUV. The fee schedule of which shall be established by an ordinance.

Section 1.07 Development Incentives

The Village of Palmetto Bay creates the following programs to incentivize development within the DUV. In order to implement the concepts and ideas fundamental to the ultimate vision, creative ways to manage and improve the limited available space within the DUV must be explored and addressed to attract the best development.

Note 4C: Define "the ultimate vision" Define "the best development"

A. The Village Parking Incentives Program

Parking Standards shall be provided on-site or off-site through a centralized parking system in order to encourage development of property, consolidate parking and implement the vision of the Village.

Define "centralized parking"

The Village of Palmetto Bay hereby creates the Village Parking Incentives Program to provide developers an opportunity for relief from parking requirements set forth in the DUV. The intention of this program is to reduce the burden that mandatory

Note 4A: What about bonus floors? Are they to be listed here too?

Note 4B: Details exist limiting TDRs. Similar details should exist for reserve residential units and stories.

parking requirements have on property owners. These opportunities are as follows:

1. Proximity to Premium Transit: To encourage development of sites around transit stations, a 30% reduction of the total parking requirements of this code shall be permitted for all sites within 1,000 feet of a premium transit station.

Note 5A: Identify which properties are within this distance

Note 5B: Consider removing office from ground floor as part of this incentive since goal is to achieve retail/restaurants. Furthermore, identify areas where retail on the ground floor is mandatory.

Note 5C: PILOP needs to have receiving site identified

2. Ground Floor Mixed-Use: To encourage mixed-use developments within the DUV, a reduction of 20% of the total parking requirements of this code shall be permitted for development that is mixed-use, with the total area of the development at the ground floor (1st Story) one (1) use (Commercial-Retail, Office,) and the remainder of stories above, another use (Office, Residential).

3. Payment-in-Lieu: Developments within the Downtown Village (DV) and Downtown General (DG) Sectors, Figure 2, may provide payment-in-lieu of one (1) parking space for every three (3) parking spaces required by the total parking requirements of this code. The fee of which shall be established by an ordinance and payable to the Village Centralized Parking Trust Fund.

Developments meeting more than one (1) of the qualifications of the parking incentives program shall be permitted to combine multiple reductions; the combined reduction shall be calculated by applying the reductions in the order of applicable criteria above.

B. The Village Landscape and Open Space Improvement Program

It is the vision of the Village of Palmetto Bay to transform into a vibrant, attractively developed Downtown Urban Village (DUV), with a network of pedestrian and bike-friendly, open spaces. The Village of Palmetto Bay hereby creates the Village Landscape and Open Space Program to incentivize development that supports this ultimate vision.

1. Developments that seek relief from the minimum standards for private open space on the site may dedicate an exterior area of their parcel, adjacent to a street frontage, as a public open space. In exchange, developments shall be permitted a reduction of the total private open space requirements for their development, equal to the area designated as public open space. The maximum total reduction shall be a 30% reduction of private open space.

Note 5D: Revise. How is public open space provided, who uses it, shape, etc

5I: This appears to be "double-dipping" and provides no benefit to the overall community

Note 5E: this fee is insignificant in the scheme of a development. Consider alternate requirement

2. Developments that seek relief from the minimum standards for tree requirements on the site may provide payment-in-lieu, up to 35% of the minimum required trees, according to Sec.4.04 A-E 6. The fee of which shall be established by an ordinance and payable to the Village Landscape and Open Space Improvement Trust Fund.

Section 1.08 Green Certification

All development projects within the Village of Palmetto Bay

shall achieve baseline third-party certification, such as LEED, ENERGY STAR for Buildings, National Green Building Standard, or other similar organizations.

Section 1.09 Development Bonuses

The intent of the Development Bonuses program is (1) to provide development design options that contribute to the overall quality of a project, and/or (2) to incentivize future development to contribute to the construction of amenities that provide a public benefit within the downtown area. The program involves standards that exceed minimum required development parameters. Bonuses come in the form of additional building height (stories), not to exceed the maximums as provided for in the eligible districts.

A. Eligibility Note 5F: Identify graphically

Development parcels may be eligible for development bonuses within the DUV as follows:

- All developments within the Downtown Village (DV) and Downtown General (DG) Sectors
- All development parcels, minimum 30,000 sq.ft. lot area, with primary frontage along SW 97th Avenue (Franjo Road) in the Urban Village (UV) Sector.

Note 5G: This encourages land consolidation in a transitional area, inconsistent with preservation of appropriate scale and character.

B. Development Bonus Opportunities

The following bonuses shall be available for eligible development parcels:

1. Parking Bonus

Note 5H: Underground parking, which is very expensive, undertaken only to meet high minimum parking requirements, should not be used as a justification for bonuses that lead to excessive building height and out of scale developments. Consider right-sizing minimum parking requirements instead."

- (a) Developments with underground parking shall be permitted an increase of one (1) story, not to exceed maximum overall bonus building height, as prescribed within each of the sectors. To qualify, the amount of underground parking spaces shall equal a minimum of the average number of spaces on all parking levels combined.

- (b) Developments that contribute a fee equal to 15% of their adjusted required parking amount shall be permitted an increase of one (1) story, not to exceed maximum overall bonus building height, as prescribed within each of the sectors. This fee shall not be payment-in-lieu of required parking. The fee shall be established by ordinance and payable to the Village Centralized Parking Trust Fund.

2. Landscape and Open Space Bonus

- (a) Developments that contribute a fee in an amount equal to the required park impact fee, shall be permitted an increase of one (1) story, not to exceed the maximum overall bonus building height, as prescribed within each of the sectors. This fee shall does not replace the required park impact fee. The fee shall be established by an ordinance and payable to the Village Landscape and Open Spaces Trust Fund.

- (b) Developments that construct the public open spaces identified on the Public Open Spaces Plan

2.04, shall be permitted an increase of one (1) story, not to exceed maximum overall bonus building height, as prescribed within each of the sectors.

3. Green Bonus

- (a) Development projects that exceed the baseline third-party green certification level shall be permitted an increase of one (1) story, not to exceed the maximum overall bonus building height, as prescribed within each of the sectors.

C. General

Development projects shall be permitted to combine bonuses, not to exceed the maximum overall bonus building height, as prescribed within each of the sectors. All development bonuses shall be permitted pending the approval of the Village Council.

Note 6A: How does this work? A 2,400sf open space equals an entire floor? How about a 4,800sf? or a 70,000sf one?

If the open spaces are being built as a result of the reduced parking requirement (an incentive) are they allowed to "count again" for a story?

This is a tangible benefit to the community but needs to be better quantified and more appropriately valued.

Note 6B: Not a tangible benefit to the residents. Too basic to add an entire floor. It could be used to provide a density bonus (if at all).

Note 6C: Define maximum overall bonus

Note 6D: This language is contradictory and may lead to confusion. Here it is stated that all bonuses shall be permitted pending approval. What does this entail?

Section 105.C.4 states that the Village Council may grant, grant with conditions or deny reserve residential units, reserve commercial units or TDRs.

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Village of Palmetto Bay Illustrative Vision Plan



30-50.23.2 REGULATING PLANS

Section 2.01 Purpose

This section establishes the Regulating Plans which define the district wide regulations that foster development on the properties within the Village of Palmetto Bay Downtown Urban Village (DUV). The information within each of the regulating plans, in addition to the standards in Sec.3-6 make up the Village of Palmetto Bay DUV. The Regulating Plans consist of the following plans:

- Sector Plan
- New Streets Plan
- Public Open Spaces Plan
- Street Hierarchy Plan
- Residential Density Plan

Note 9A:
If the Village is seeking a specific character, why not use well defined building types (as opposed to "flexible types")

Section 2.02 Sector Plan

The Sector Plan, Figure 2, divides the areas within the Downtown Urban Village (DUV) into sectors that progress from urban, more intense type of development to lesser urban areas, all of which provide for a mixture of uses.

Each of the sectors directly reinforce the Village of Palmetto Bay's vision for the Downtown Urban Village (DUV) and provide an opportunity for property owners to develop in a manner that supports these urban design objectives and transforms the urban realm into a cohesive, mixed-use downtown area.

The sectors within the Downtown Urban Village (DUV) allocate land uses, building type, and frontage type within a planned area, in addition to detailed parameters for building placement, form and height.

A. Downtown Village (DV)

The Downtown Village (DV) sector applies to the primary area of the Downtown Urban Village (DUV), which is composed of the most vital, concentrated, ped/bike-oriented areas and defined by multi-story flexible block and flex building typologies.

The flexible block and flex building typologies are suitable to satisfy the broad assortment of retail, office, light service and residential uses that serve a true mixed-use downtown area. Higher intensity commercial uses may line the street front at ground level with offices or multi-family residential units above. **Note 9B: Not reflective of intent. there are areas where ground floor retail should be mandatory.**

Landscape should reflect the urban character of the sector, emphasizing ped/bike protection and accentuating the architectural character of the area, by planting in tree grates or landscape islands.

Parking is permitted both on-site and off-site within the DV

Sector.

B. Downtown General (DG)

The Downtown General (DG) sector applies to the area of the Downtown Urban Village (DUV) immediately surrounding the Downtown Village (DV). The Downtown General (DG) serves as a transition from the large properties abutting US1 into the area intended to become the main, ped/bike-friendly, transit-connected Downtown Urban Village (DUV).

This sector provides for flexible building types in the general form of flexible blocks and flex buildings, both of which may accommodate higher intensity commercial/retail at the ground level and offices or multi-unit residential on the floors above. More traditional 'urban big box' commercial uses may be accommodated in the Downtown General (DG) sector with a selection of other compatible uses, vertically integrated within the same building.

Landscaping should consist of a more urban scale and pattern of planting with street trees planted in tree grates and landscape islands.

Parking is permitted both on-site and off-site within the DG Sector.

C. Urban Village (UV)

The Urban Village (UV) sector is applied to lower intensity mixed-use and residential area within the Downtown Urban Village (DUV). This sector serves as a transition from the higher intensity downtown sectors, by prescribing additional lesser intense building types.

Note 9C: What is the mixed-use rowhouse?

The flex and rowhouse (mixed-use) building type maintain a compatible use profile to the other more intense districts, but establishes compatibility in scale with a residential rowhouse typology introduced in the Urban Village (UV) sector.

Landscaping should be consistent with the more neighborhood scale of the district with shade trees planted in tree grates, landscape islands and planting strips with some shallow-depth landscaping in the setbacks separating some building entrances from the public sidewalks.

Parking is permitted both on-site and off-site within the UV Sector.

D. Neighborhood Village (NV)

The Neighborhood Village (NV) sector is applied to the existing lower intensity areas within the Downtown Urban Village (DUV) but is meant to introduce a mixed-use component compatible with the development of the higher intensity mixed-use sectors.

Rowhouse typologies are introduced, with the ability to provide a mixed-use component to the district but remain compatible with higher intensity residential typologies like Stacked Apartment buildings. Single-Family houses may be developed as a more urban typology like sideyard and courtyard houses, which maintain the street edge and continue to respect the public realm.

Note 10A: Is the rowhouse (typically a residential type) used as a residential unit or a mixed use type?

Landscaping should be consistent with the neighborhood scale of the district with shade trees planted in landscape islands or planting strips and some shallow-depth landscaping in any setbacks separating building entrances and frontage features from the public sidewalks.

Parking is permitted both on-site and off-site within the NV Sector.

Key

(DV)	Downtown Village	
(DG)	Downtown General	
(UV)	Urban Village	
(NV)	Neighborhood Village	

Note 10A: The zone-boundaries need to be addressed. They either need to be redefined or the regulatory framework needs to include more detail (or both).
 Proper transition between zones is missing.
 This plan also depicts the highest possible densities and intensities directly adjacent to the lowest prescribed ones.
 There are key transitions are happening along the road as opposed to occurring at the center of the block.
 There are areas colored differently, yet the code allows for the same building types (e.g. UV & DG)

Figure 2

Sector Plan



Section 2.03 New Streets Plan

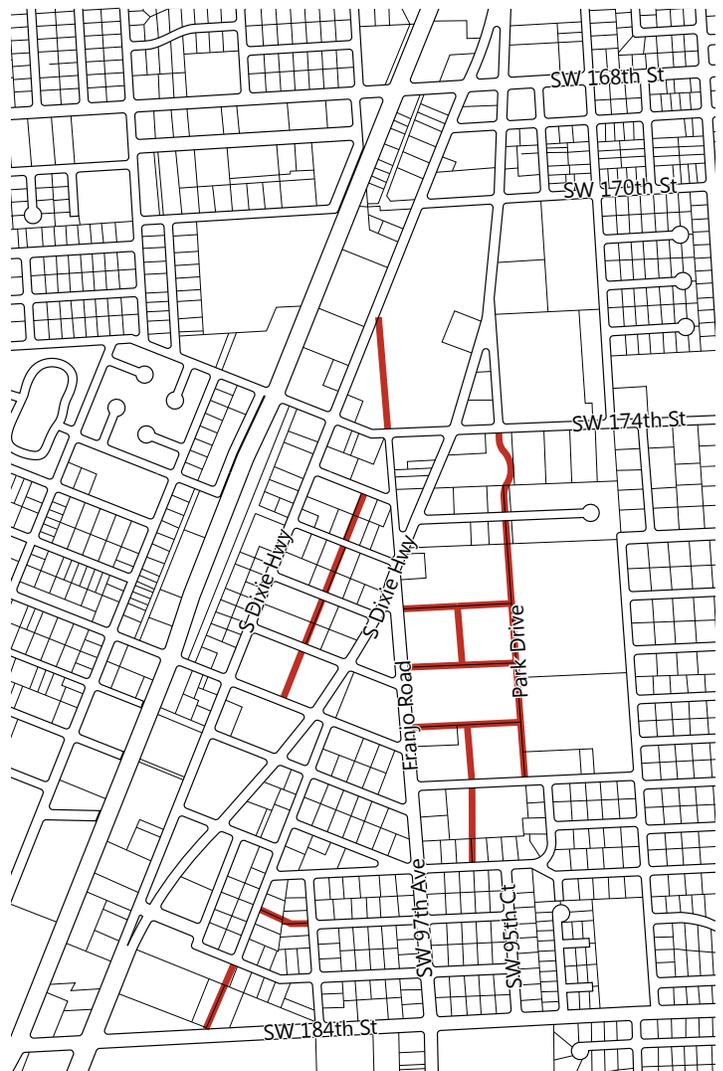
The New Streets Plan, Figure 3, shows the location and number of new streets needed to create the improved network of streets prescribed by the Village of Palmetto Bay Illustrative Vision Plan. All new streets shall be located in the same general location as shown in the New Streets Plan and developed under the standards established by these regulations.

Key
New Street



Figure 3

New Streets Plan



Section 2.04 Public Open Spaces Plan

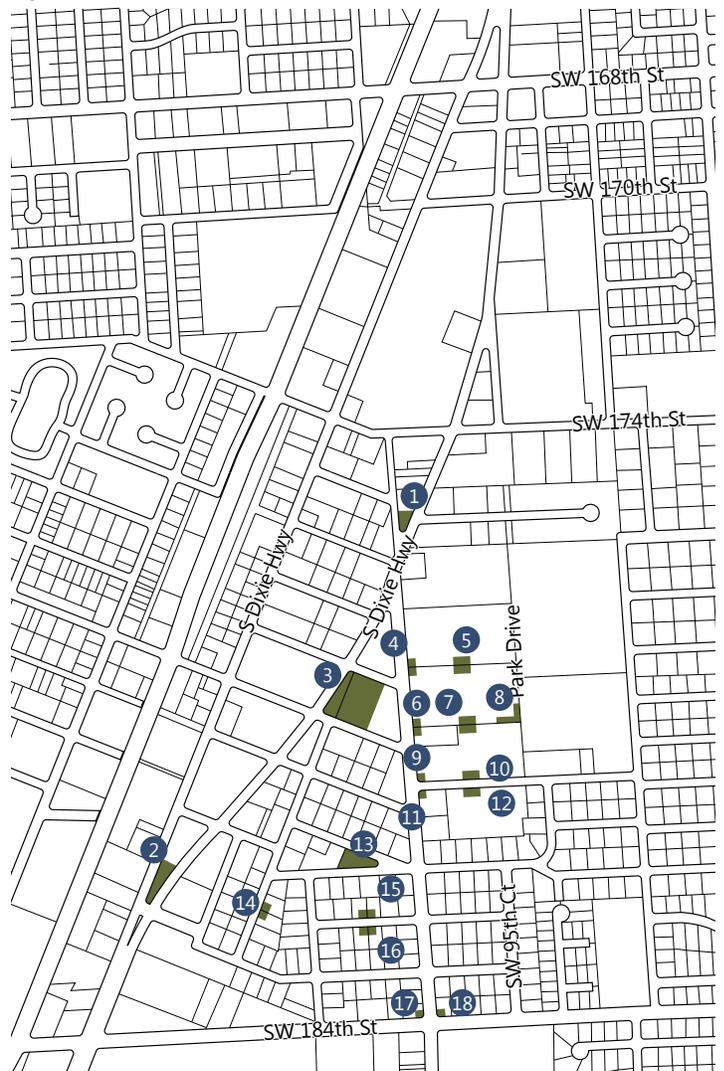
The Public Open Spaces Plan, Figure 4, shows the number and location of public open spaces proposed to create an improved network of open spaces within the future vision of the Village of Palmetto Bay. The general proportion and size of each public space shall be controlled by Table 1.

Key
Public Open Space

Table 1 Open Space Areas

Open Space	Area
1	7,500 sq.ft.
2	14,000 sq.ft.
3	70,000 sq.ft.
4	4,800 sq.ft.
5	9,600 sq.ft.
6	4,800 sq.ft.
7	9,600 sq.ft.
8	8,000 sq.ft.
9	2,400 sq.ft.
10	4,800 sq.ft.
11	2,400 sq.ft.
12	4,800 sq.ft.
13	16,000 sq.ft.
14	4,800 sq.ft.
15	4,800 sq.ft.
16	4,800 sq.ft.
17	2,400 sq.ft.
18	2,400 sq.ft.

Figure 4 Public Open Spaces Plan



Section 2.05 Street Hierarchy Plan

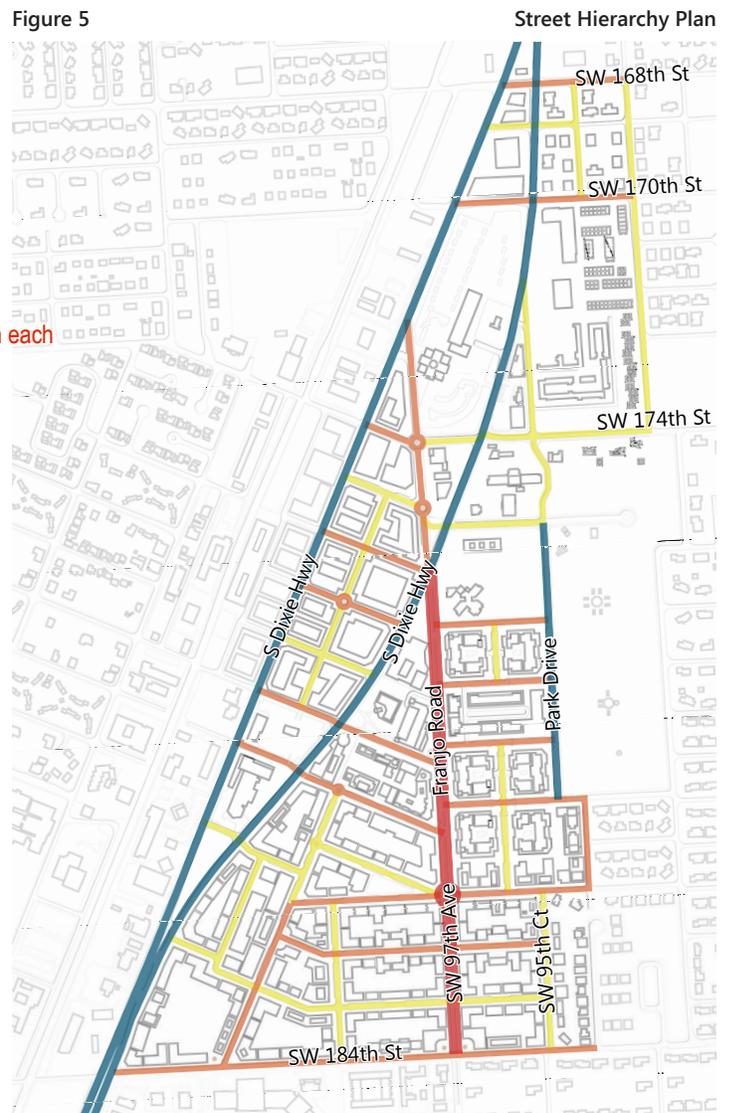
The Street Hierarchy, Figure 5, plan illustrates the types of streets, both existing and new, to be constructed/redeveloped within the Village of Palmetto Bay. Streets designed according to the standards within these regulations contain many new character elements that will contribute to the improved street network and ped/bike character of the Downtown Urban Village (DUV).

For all street types, a build-to line shall be established consistent with the street type that is identified in Figure 62 and the corresponding standards illustrated in Sec.5.01 A-E. For the stoop and porch frontage types in Sec.4.05 D-E, within the Urban Village (UV) and Neighborhood Village (NV) sectors, the setbacks shall be 10 feet.

The following streets and corresponding Figure 5, outline the hierarchy of streets from top priority down. This hierarchy of streets is important for development where frontage and access shall be considered.

- Key**
- Priority 'A' Street
 - A Street
 - Priority 'B' Street
 - B Street

Note 13: Incomplete description of A & B streets. How should buildings behave in each situation. What is the difference between Priority A and A? Priority B and B?



Section 2.06 Residential Density Plan

The Residential Density Plan, Figure 6, illustrates the range of densities that shall be permitted on the parcels within the Downtown Urban Village (DUV). All densities shall be based on the gross lot area, meaning that parcels shall be extended to the center line of the street for the purpose of calculating the lot area.

- A. **Minimum Average Unit Size:** In total, a mixture of unit sizes and types shall be provided in all residential components of development. The number of units in a multi-family building to be constructed in the Downtown Urban Village (DUV) shall meet the minimum average required unit size of 750 sq.ft. minimum. This will encourage development of mainly one (1), two (2) and three (3) bedroom residential units. The minimum unit size for any residential units that shall be permitted within the DUV is 625 sq.ft. min. **Note14A: The density plan appears to be inconsistent with the zones as outlined in the sector plan. this language seems to imply that there need to be different unit sizes and different building types in each development (?) Clarify. What is a unit type mixture? 625 for a 1 bedroom? Why dictate size of units?**

Key

- 24 du/ac max. base density (gross)
- 14 du/ac max. base density (gross)

Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve residential units and/or TDR residential units.

Table 2 Minimum Area of Multi-Family Units

Multi-Family Units	Area (min.)
Studio	625 sq.ft.
1 Bedroom	650 sq.ft.
2 Bedroom	850 sq.ft.
3 Bedroom	1,100 sq.ft.

Note 14B: TDR process and Reserve Unit system should be defined via separate ordinance

Figure 6 Residential Density Plan



Section 2.07 Uses

No land, body of water or structure shall be used or permitted to be used, and no structure shall be hereafter erected, constructed, reconstructed, moved, structurally altered, or maintained for any purpose in the Downtown Urban Village (DUV), except as provided in this section. The uses delineated herein shall be permitted only in compliance with the regulating plans and general requirements provided in this section.

- A. **Residential Uses:** Residential uses are permitted in the areas designated in the Sector Plan as Downtown Village (DV), Downtown General (DG), Urban Village (UV) and Neighborhood Village (NV), as provided in Table 3.
- B. **Ancillary Uses:** The following uses shall be permitted as

Table 3 Residential Uses

Sectors				Residential Uses
DV	DG	UV	NV	
○	○	○	●	Detached single-family dwelling Attached single-family dwelling Multiple-family dwelling unit
○	○	●	●	
●	●	●	●	

Key
 Permitted ●
 Non-Permitted ○

ancillary uses to a lawful residential units in the areas designated:

1. Urban Village (UV) and Neighborhood Village (NV) sectors:
 - (a) The following accessory buildings and non-residential uses, when located in the rear yard: workshop, garage, utility shed, gazebo, cabana, garden features, basketball hoop, pool and carport;
 - (b) For an attached or detached single-family dwelling, a single accessory dwelling unit with a maximum of 600 square feet of habitable building space under the same ownership as the single-family unit;
 - (c) For Home office, as provided in Section 30-60.14-Home Office of the Village of Palmetto Bay Code.

Note 15: Identify areas where MU is mandatory.

- C. **Mixed Uses:** The vertical or horizontal integration of two or more of residential, business and office, civic and institutional uses may be required as provided herein. Vertical integration allows any combination of primary uses, with commercial/retail uses typically located on the ground floor and office and/or residential uses on the upper floors. Horizontal integration allows any combination of parcels with different primary uses within the same block under the same ownership. Vertical integration of mixed-uses shall be required within buildings that have primary frontage along SW 97th Avenue (Franjo Road).
 1. The following non-residential uses shall be permitted

in the areas designated in the Sector Plan as Downtown Village (DV), Downtown General (DG), Urban Village (UV) and Neighborhood Village (NV), as provided in Table 4 and provide no outside storage and/

Table 4 Non-Residential Uses

Non-Residential Uses	Sectors			
	DV	DG	UV	NV
Civic Uses	●	●	○	○
Religious Facilities	●	●	○	○
Schools (K-12)	●	●	○	○
Municipal Recreation	●	●	●	●
Group Residential Home	●	●	●	●
Big-Box Retail/Service	●	●	○	○
General Retail/Personal Service	●	●	●	○
Neighborhood Proprietor Commercial-Retail/Office and Services	●	●	●	●
Automotive Uses	○	○	○	○
Gas/Service Stations	○	○	○	○
Office Uses	●	●	●	○
Colleges and Universities	●	●	●	○
Entertainment Uses	●	●	○	○
Accommodation Uses	●	●	●	○
Food Beverage Establishments	●	●	●	○
Drive-Through Facilities	○	○	○	○
Commercial Parking Structure	●	●	○	○

Key
 Permitted ● Non-Permitted ○
 Permitted with provision Sec.2.07 D ○

or display of merchandise, equipment, materials or supplies:

- D. Supplementary to Table 4, the following uses shall be permitted provided the following:
 1. Civic uses on sites that are less than one (1) acre shall be permitted within the Urban Village (UV) and Neighborhood Village (NV) sectors
 2. Automotive uses shall be permitted within the Downtown General (DG) sector subject to the following conditions:
 - (a) Used sales shall only be permitted in conjunction with new sales; and
 - (b) Ancillary sales, service and repair shall only be permitted in conjunction with new sales; and
 - (c) No outside storage and/or display of merchandise, equipment, materials or supplies is permitted.
 3. Legally established, presently operating gas stations shall continue to operate as legal but non-conforming and subject to the standards of Sec.1.04
 4. Neighborhood Proprietor Commercial-Retail/Office and Services shall be permitted in the Neighborhood Village (NV) Sector and shall not exceed 20% of the buildings square footage and shall occur at the ground level.
 5. Drive-through facilities shall:

- (a) Be permitted only in the Downtown General (DG) sector;
 - (b) Provide a continuous street façade consisting of buildings or walls along all rights-of-way except driveways. When provided, walls shall not exceed three and one-half (3 1/2ft) feet in height and shall be a minimum of 75% opaque. The main building shall provide a minimum of 40% of building frontage along the primary frontage; and
 - Note 16A: Even on primary A streets? Usually walls are not allowed on A steets.
 - (c) Be permitted upon determination that the drive-through shall create minimal traffic congestion or disruption to adjacent streets.
 - Note 16B: Who determines the intensity of the impact? What if the user changes?
6. Sidewalk cafe and outdoor table service may be provided in compliance with the Village's Sidewalk Café Ordinance. See Sec.30-60.17.
 7. Any other uses not specifically listed are prohibited.

30-50.23.3 Urban Design Standards

Section 3.01 Purpose

This section identifies the Urban Design Standards for the development in each of the sectors and ensures that said development is consistent with the vision for the Village of Palmetto Bay Downtown Urban Village (DUV). Development within each sector shall be in compliance with the standards for building types, building form and parking. The sectors are organized by the most urban Downtown Village (DV) and Downtown General (DG) to the least, Urban Village (UV) and Neighborhood Village (NV). Unless otherwise stated, all standards in this section are expressed as 'minimums' and may be exceeded in compliance with all applicable provisions of the zoning code.

Note 17: What is the difference between building type and building form? Furthermore, this section states that developments SHALL be in compliance, so why were "design considerations" provided relative to building types/form/parking?

Section 3.02 Summary of Sectors

Below, in Table 5, the four (4) sectors organized from most urban to least urban in descending order, according their role in the Village of Palmetto Bay Downtown Urban Village (DUV) and as they appear in this Section.

- A. Downtown Village (DV)
- B. Downtown General (DG)
- C. Urban Village (UV)
- D. Neighborhood Village (NV)

Table 5

Summary of Sectors

DV	DG	UV	NV
Downtown Village	Downtown General	Urban Village	Neighborhood Village
Building Types	Building Types	Building Types	Building Types
Flexible Block ●	Flexible Block ●	Flexible Block ●	Flexible Block ○
Flex Building ●	Flex Building ●	Flex Building ●	Flex Building ●
Rowhouse ○	Rowhouse ○	Rowhouse ●	Rowhouse ●
Stacked Apartment ○	Stacked Apartment ○	Stacked Apartment ●	Stacked Apartment ●
Single Family House ○	Single Family House ○	Single Family House ●	Single Family House ●
Frontage Types	Frontage Types	Frontage Types	Frontage Types
Arcade ■	Arcade ■	Arcade ■	Arcade □
Storefront ■	Storefront ■	Storefront ■	Storefront ■
Forecourt ■	Forecourt ■	Forecourt ■	Forecourt ■
Stoop □	Stoop □	Stoop ■	Stoop ■
Porch □	Porch □	Porch □	Porch ■

Key
 Permitted ●
 Non-Permitted ○
 Allowed ■
 Non-Allowed □

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A. Downtown Village (DV)

Sector Summary

Note 19A: This code doesn't appear to differentiate between the DV (Downtown Village) and DG (Downtown General). They appear to be different "in name only." The Village character is lost with heights equal to those on the Island. Furthermore, this area is directly adjacent to single-family fabric. Bonus height should be limited to one additional story, provided that no portion of the development higher than 3 stories when adjacent to Urban Neighborhood boundaries.



Building Types	Lot Size W x D	Residential Density*	Building Height	Uses by Story	Private Open Space
Flexible Block	160'x160' (min.)	24 du/ac	3 stories (min.) 5 stories (max.) 8 stories (with bonus)	1st 2nd+ <i>Define streets where retail (and not O/R) is mandatory in 1st floor.</i>	15% of site
Flex Building	80'x100' (min.)	24 du/ac	3 stories (min.) 5 stories (max.) 8 stories (with bonus) <i>(see NOTE 19).</i>	1st 2nd+ <i>Define streets where retail (and not O/R) is mandatory in 1st floor.</i>	15% of site

*Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve and/or TDR units.

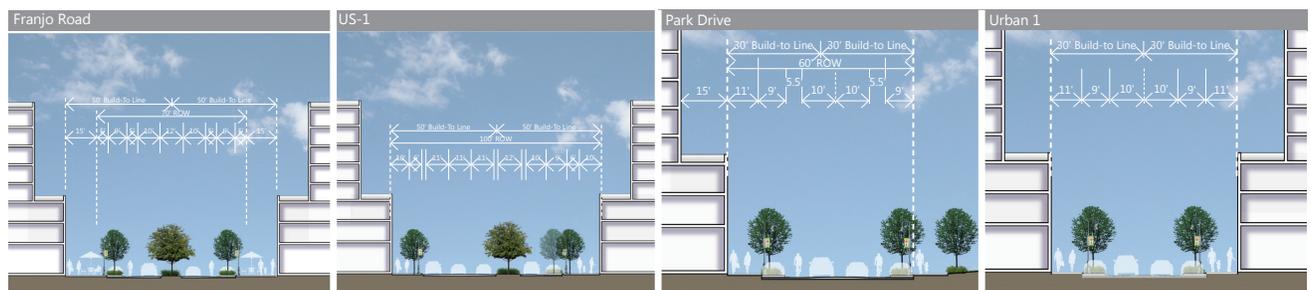
Streets and Building Placement

Street Type	ROW	Build-To Line Primary	Build-To Line Secondary	Sidewalk	Bike Lanes	Uses (at Street)	Glazing (at Street)
Franjo Road (FR)	70'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	20'	Yes	C-R	70% (min)
US-1 (US1)	100'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	16'	N/A	C-R O R	70% min. (C-R/O only)
Park Drive (P)	60'	30' (from centerline of road, up to 2 stories)	45' (from centerline of road, >2 stories)	10'	Yes	C-R O R	70% min. (C-R/O only)
Urban 1 (TS-U1)	50' or 60'	30' (from centerline of road, up to 2 stories)	45' (from centerline of road, >2 stories)	10'	N/A	C-R O R	70% min. (C-R/O only)

*Why "jog" if building height is different?
Why regulate glazing independent of use? Glazing is better addressed at the building type level.
Comment/questions apply to entire table.*

NOTE 19: The Downtown Village (DV) will set the character for the community. It is the area identified in the code as the center of the community, where residents and visitors alike will go to dine, shop, find entertainment, as well as live and work. However, the regulatory framework doesn't differentiate this area to be the most unique and special within the Village from the DG except in that it defines a lower "minimum" height. Minimums don't define differences relative to space, character or use. The Village needs to clearly define its objective for this area and the bonuses (if) offered, should be reflective of those goals. Is the intent to develop a vibrant Village center? Then bonuses (if) offered should further this principle. Is the intent to maintain a smaller-scale feel? Then the form should respond accordingly. The regulatory language for this zone, like all others, should include appropriate transition to other zones, in particular those that are less dense/intense.

Key: Commercial-Retail: C-R Office: O Residential: R



1. Building Types and Height

Table 6 identifies the permitted building types and the minimum and maximum heights allowed, by building type within the Downtown Village (DV) sector, subject to compliance with all other applicable standards.

Note 20 A: As this form-based code appears to be organized around building types (a perfectly acceptable method) consider including a section that clearly and graphically depicts the building types permitted.

Table 6 Building Types and Heights (Stories)

Building Types		Min.	Max.	Max. w/ Bonus
(a) Flexible Block	●	3	5	8 ^{i,ii}
(b) Flex Building	●	3	5	8 ^{i,ii}
(c) Rowhouse	○	-	-	-
(d) Stacked Apartment	○	-	-	-
(e) Single Family House	○	-	-	-

Key
 Permitted ●
 Non-Permitted ○
 N/A -

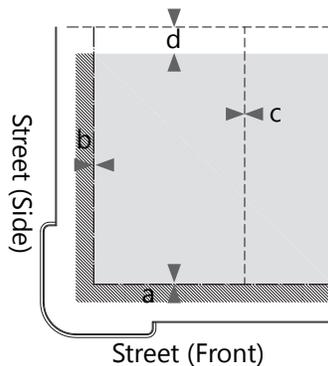
- (i) For buildings abutting Park Drive and overlooking Palmetto Bay Park maximum height shall be 4 stories, and, shall be limited to six (6) stories, with bonus, to provide transition to the park and neighborhoods to the East.
- (ii) Unless primary frontage is along Franjo road, buildings adjacent to a Neighborhood Sector (UV) or Neighborhood Village (NV), maximum height, with bonuses, shall be limited to six (6) stories to provide compatibility with the neighborhood scale.

Note 20B: Building transitions should be better specified. A potential 5-story difference between one use and the other seems excessive. A more granular approach to the Sector Map may be necessary to achieve appropriate transitions.

2. Building Setbacks

Figure 7 and corresponding Table 7 identifies any required setbacks, for the (DV) sector. The required setbacks shall apply to all stories of a building, at the ground level. Setbacks shall be measured from the build-to line along street frontages and the property line for all other sides.

Figure 7 Building Setbacks



Key
 Development Area ■
 Encroachment Area ▨
 Build-to Line - -
 Property Line - -

Table 7 Building Setbacks

Required Setbacks	
(a) Street (Front)	0ft
(b) Street (Side)	0ft
(c) Side (Interior)	0ft min.
(d) Rear	15ft min.

3. Frontage Requirements

All ground floors of building types within the (DV) sector shall comply with the allowed frontage types in Table 8, subject to compliance with all other applicable standards. Refer to Sec.4.04 A-E 7(b) for permitted frontage type by individual building types.

Table 8 Frontage Types

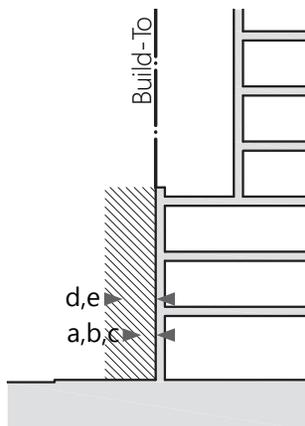
Frontage Types	
(a) Arcade	■
(b) Storefront	■
(c) Forecourt	■
(d) Stoop	□
(e) Porch	□

Key
 Allowed ■
 Non-Allowed □
 N/A -

4. Encroachments

Figure 8 and corresponding Table 9 identifies the encroachments allowed into the build-to line, and the vertical clearance and horizontal projection by encroachment type, within the (DV) sector.

Figure 8 Encroachments



Key
 Encroachment Area (hatched pattern)
 Build-to Line (dashed line)

Table 9 Encroachments

Encroachments	Vertical Clearance (Ground)	Horizontal Projection
Street (Front and Side)		
(a) Signage ⁱ	8ft min.	24" max.
(b) Lighting ⁱ	8ft min.	24" max.
(c) Awning	10ft min.	6ft max.
(d) Balcony	12ft min.	6ft max.
(e) Window	12ft min.	4ft max.
Side (Interior)	N/A	N/A
Rear	N/A	N/A

- (i) Horizontal projection of encroachment can exceed the maximum allowable distance at a rate of six (6") inches for every foot above the eight (8ft) feet min. vertical clearance, for a total of 48" max.
- (ii) Sidewalk cafes and outdoor table service may be encroach at the sidewalk level, provided that it shall be in compliance with the Village's Sidewalk Café Ordinance. See Sec.30-60.17.

5. Parking Access and Setbacks

All off-street parking and associated access that does not occur within a parking structure, in the (DV) sector, shall be developed according to the requirements provided in Figure 9-10 and accompanying Table 10-11.

Figure 9 Parking Access and Setbacks

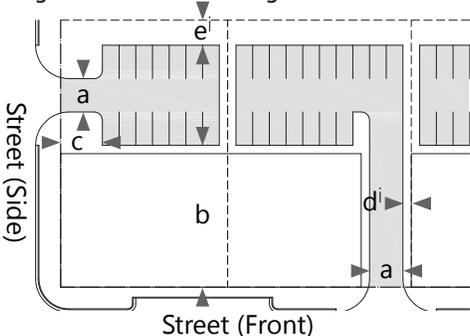


Table 10 Driveway Dimensions

Parking Access (Driveway)	
(a) 1-way	10ft min.
(a) 2-way Parking	20ft min. Not Allowed

Figure 10 Shared Access

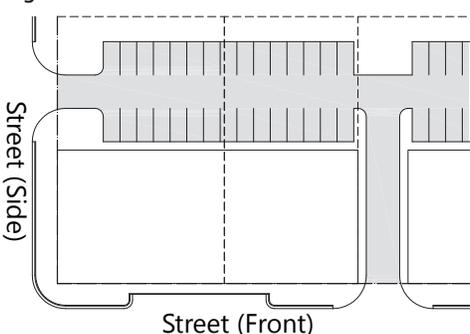


Table 11 Parking Area Setback

Required Setbacks	
(b) Street (Front)	30ft min.
(c) Street (Side)	10ft min.
(d) Side (Interior) ⁱ	5ft min.
(e) Rear ⁱ	5ft min.

- (i) Side (Interior) and Rear setback shall be min. five (5) feet and landscaped with ground cover and a low hedge/wall or fence, where driveway access is provided to the rear of the lot. See Sec.4.03 B 1(ai-ii)
 Shared access to parking shall be encouraged to limit the frequency of curb cuts along the primary frontage street, which maintains the improved streetscape and street front at the pedestrian level. See Sec.4.03 B 2. A cross access agreement between property owners shall be provided to the Village of Palmetto Bay depicting shared access.

Key
 Parking Area (hatched pattern)
 Build-to Line (dashed line)
 Property Line (solid line)

Note 21:
 - Define if surface parking can occur along an "A" street.
 - Define what happens with garage structures, how they address the public realm and how they transition to adjacent (different) zones.

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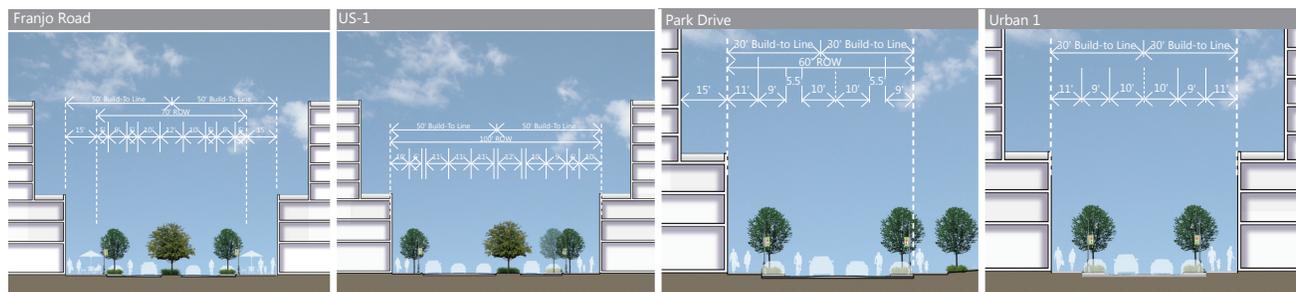
B. Downtown General (DG)

Sector Summary

DG Downtown General							
	Building Types	Lot Size W x D	Residential Density*	Building Height	Uses by Story		Private Open Space
	Flexible Block	160'x160' (min.)	24 du/ac	4 stories (min.) 5 stories (max.) 8 stories (with bonus)	1st 2nd+	C-R/O/R C-R/O/R	15% of site
	Flex Building	80'x100' (min.)	24 du/ac	4 stories (min.) 5 stories (max.) 8 stories (with bonus)	1st 2nd+	C-R/O/R C-R/O/R	15% of site
*Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve and/or TDR units.							
Streets and Building Placement							
Street Type	ROW	Build-To Line Primary	Build-To Line Secondary	Sidewalk	Bike Lanes	Uses (at Street)	Glazing (at Street)
Franjo Road (FR)	70'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	20'	Yes	C-R	70% (min)
US-1 (US1)	100'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	16'	N/A	C-R O R	70% min. (C-R/O only)
Urban 1 (TS-U1)	50' or 60'	30' (from centerline of road, up to 2 stories)	45' (from centerline of road, >2 stories)	10'	N/A	C-R O R	70% min. (C-R/O only)

NOTE 23: The Downtown Village (DG) defines the impression most visitors and passers-by will have of the Village. It's general location is along roads serving more intense transportation needs. As such, buildings and uses along it need to be more resilient. Properly placed and designed buildings with increased heights, density and intensity respond well to these more intense ROW's. If there is a place for higher density/intensity in the Village it is the DG. If the Code's intent for this area is to provide density that supports ridership (TOD) and local businesses, then the regulatory framework and bonuses (if) provided need to be tailored to this objective. If there is a concern that these buildings will generate more traffic congestion, these buildings should be designed to reduce that impact. Minimum parking allowances and penalties for exceeding those should be considered.

Key: Commercial-Retail: C-R Office: O Residential: R



NOTE 23B: If a transit-supportive area is to be successful the Village needs to work with the County to define the location of the future transit stop. Concurrently, the Village needs to define local transportation system that serves the community and feeds the larger system. As proposed the DG zone is 1.25 miles long. Even if a station were centrally located, the distance would prove too long for people to effectively walk to a station.

NOTE 23C: Bonus stories should be graphically depicted in a different manner as they are not a given. Additionally, once the objective(s) is met, the Village should do away with them (as in time-limited zoning cases).

1. Building Types and Height

Table 12 identifies the permitted building types and the minimum and maximum heights allowed, by building type within the Downtown General (DG) sector, subject to compliance with all other applicable standards.

Table 12 Building Types and Heights (Stories)

Building Types		Min.	Max.	Max. w/ Bonus
(a) Flexible Block	●	3	5	8 ⁱ
(b) Flex Building	●	3	5	8 ⁱ
(c) Rowhouse	○	-	-	-
(d) Stacked Apartment	○	-	-	-
(e) Single Family House	○	-	-	-

Key
 Permitted ●
 Non-Permitted ○
 N/A -

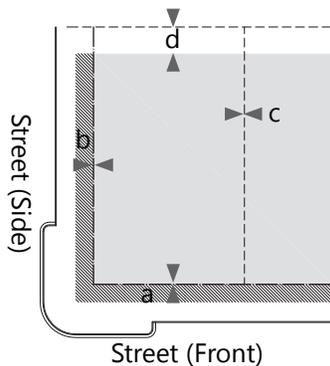
- (i) Unless primary frontage is along Franjo road, buildings adjacent to a Neighborhood Sector (UV) or Neighborhood Village (NV), maximum height, shall be 4 stories, and, shall be limited to six (6) stories, with bonus, to provide compatibility with the neighborhood scale.
- (ii) For buildings within the Island portion of the DUV, maximum height shall be 7 stories, with additional height available, up to 10 stories maximum height with bonus.

Note 24: Rather than increasing height, consider mandatory decreased parking requirements. This will have the dual effect of reducing cost of construction, therefore eliminating the need to increase the number of units and stories. If the maximum is 8, a side note should not allow two more stories.

2. Building Setbacks

Figure 11 and corresponding Table 13 identifies any required setbacks, for the (DG) sector. The required setbacks shall apply to all stories of a building, at the ground level. Setbacks shall be measured from the build-to line along the street frontages and the property line for all other sides.

Figure 11 Building Setbacks



Key
 Development Area ■
 Encroachment Area ▨
 Build-to Line ▩
 Property Line --

Table 13 Building Setbacks

Required Setbacks	
(a) Street (Front)	0ft
(b) Street (Side)	0ft
(c) Side (Interior)	0ft min.
(d) Rear	15ft min.

3. Frontage Requirements

All ground floors of building types within the (DG) sector shall comply with the allowed frontage types in Table 14 subject to compliance with all other applicable standards. Refer to Sec.4.04 A-E 7(b) for permitted frontage type by individual building types.

Table 14 Frontage Types

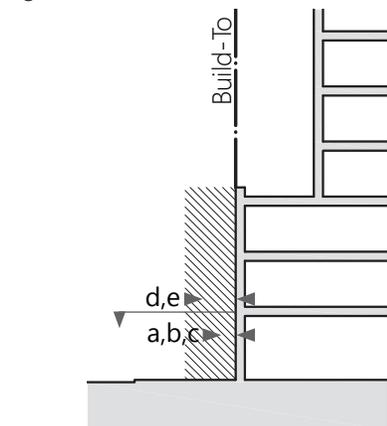
Frontage Types	
(a) Arcade	■
(b) Storefront	■
(c) Forecourt	■
(d) Stoop	□
(e) Porch	□

Key
 Allowed ■
 Non-Allowed □
 N/A -

4. Encroachments

Figure 12 and corresponding Table 15 identifies the encroachments allowed into the build-to line, and the vertical clearance and horizontal projection by encroachment type, within the (DG) sector.

Figure 12 Encroachments



Key
 Encroachment Area
 Build-to Line

Table 15 Encroachments

Encroachments	Vertical Clearance (Ground)	Horizontal Projection
Street (Front and Side)		
(a) Signage ⁱ	8ft min.	24" max.
(b) Lighting ⁱ	8ft min.	24" max.
(c) Awning	10ft min.	6ft max.
(d) Balcony	12ft min.	6ft max.
(e) Window	12ft min.	4ft max.
Side (Interior)	N/A	N/A
Rear	N/A	N/A

- (i) Horizontal projection of encroachment can exceed the maximum allowable distance at a rate of six (6") inches for every foot above the eight (8) feet min. vertical clearance, for a total of 48" max.
- (ii) Sidewalk cafes and outdoor table service may encroach at the sidewalk level, provided that it shall be in compliance with the Village's Sidewalk Café Ordinance, Sec. 30-60.17.

5. Parking Access and Setbacks

All off-street parking and associated access that does not occur within a parking structure, in the (DG) sector, shall be developed according to the requirements provided in Figure 13-14 and accompanying Table 16-17.

Note 21:
 - Define if surface parking can occur along an "A" street.
 - Define what happens with garage structures, how they address the public realm and how they transition to adjacent (different) zones.

Figure 13 Parking Access and Setbacks

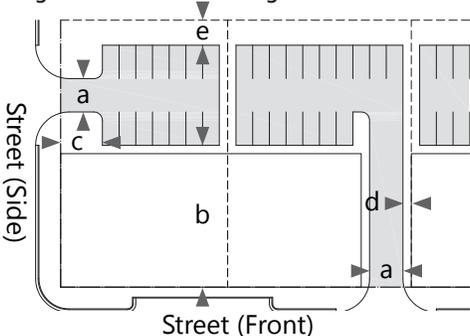
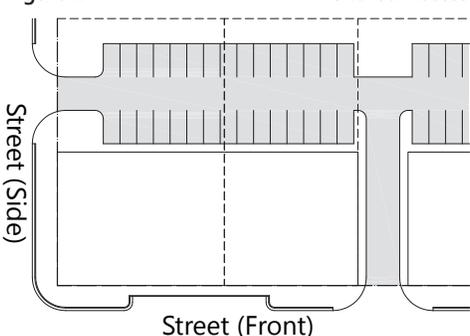


Figure 14 Shared Access



Key
 Parking Area
 Build-to Line
 Property Line

Table 16 Driveway Dimensions

Parking Access (Driveway)	
(a) 1-way	10ft min.
(a) 2-way	20ft min.
Parking	Not Allowed

Table 17 Parking Area Setback

Required Setbacks	
(b) Street (Front)	30ft min.
(c) Street (Side)	10ft min.
(d) Side (Interior) ⁱ	5ft min.
(e) Rear ⁱ	5ft min.

- (i) Side (Interior) and Rear setback shall be min. five (5) feet and landscaped with ground cover and a low hedge/wall or fence, where driveway access is provided to the rear of the lot. See Sec.4.03 B 1(ai-ii).

Shared access to parking shall be encouraged to limit the frequency of curb cuts along the primary frontage street, which maintains the improved streetscape and street front at the pedestrian level. See Sec.4.03 B 2. A cross access agreement between property owners shall be provided to the Village of Palmetto Bay depicting shared access.

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C. Urban Village (UV)*

Sector Summary

UV		Urban Village					
	Building Types	Lot Size W x D	Residential Density*	Building Height	Uses by Story		Private Open Space
	Flexible Block	160'x160'	24 du/ac	3 stories (min.) 5 stories (max.) 8 stories (with bonus)	1st 2nd+	C-R/O/R C-R/O/R	15% of site
	Flex Building	80'x100'	24 du/ac	3 stories (min.) 5 stories (max.) 6 stories (with bonus)	1st 2nd+	C-R/O/R C-R/O/R	15% of site
	Row-house	80'- 125'x100'	24 du/ac	2 stories (min.) 3 stories (max.)	1st 2nd+	C-R/O/R R	400 sq.ft. per unit
	Stacked Apartment Building	80'-200'x 100'	24 du/ac	2 stories (min.) 4 stories (max.)	1st 2nd+	R R	10% of site
Single-Family House	45'- 100'x100'	24 du/ac	3 stories (max.)	1st 2nd+	R R	10% of site	

Note 27: Implementation of the regulations here outlined will result in buildings with the same density and intensity as in the DG zone. Changing minimum building heights does not guarantee a change in scale. Particularly when maximum heights allowed are same as neighboring areas (flex block, 8 stories).

Note 27 B: Why establish minimum arbitrary quantities (open space). Setbacks and market should take care of this.

Note 27C: If Flex buildings are allowed without difference between Urban Village and Downtown Village, why use a different name? Along Franjo Rd nothing changes from DV. This zone is effectively an extension of the DV. It is not clear why neighborhood "B" streets would be allowed this intensity.

What is the difference between TS-U1 and TS-N1 in this area? Why are these denominations not identified in a map? How do they correlate to the street types identified?

Note 27D: Page 14 identifies density in this area as 14du/ac. Table on this page identifies at 24du/ac. Which one is it?

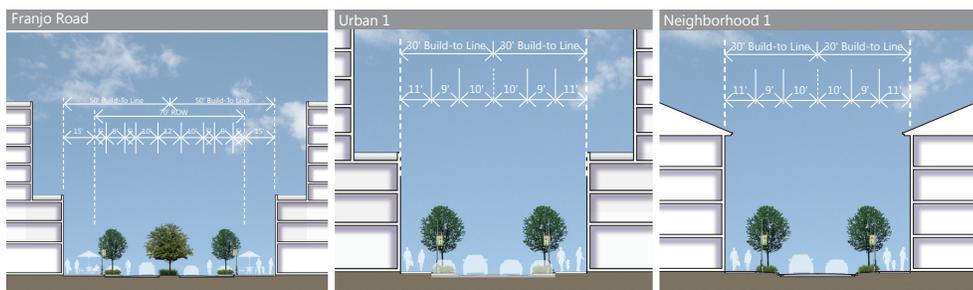
What is the difference between Urban 1 and Neighborhood 1? Roads are same width. Why are buildings not stepping back? Why is the most urban "base" proposed for the area adjacent to the least intense neighborhood?

*Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve and/or TDR units.

Streets and Building Placement

Street Type	ROW	Build-To Line		Sidewalk	Bike Lanes	Uses (at Street)	Glazing (at Street)
		Primary	Secondary				
Franjo Road (FR)	70'	50' (from centerline of road, up to 2 stories)	65' (from centerline of road, >2 stories)	20'	Yes	C-R	70% (min)
Urban 1 (TS-U1)	50' or 60'	30' (from centerline of road)	45' (from centerline of road)	10'	N/A	C-R O R	70% min. (C-R/O only)
Neighborhood 1 (TS-N1)	50' or 60'	30' (from centerline of road)	45' (from centerline of road)	10'	N/A	R	N/A

Key: Commercial-Retail: C-R Office: O Residential: R



(*) Notwithstanding any provision of this code amendment or the Downtown Urban Village Ordinance, any land, development applications or permit requests regarding properties within the Neighborhood Village or Urban Village, which are currently (as of January 4, 2016, which is the date of passage of this amendment on second reading) constructed as single family residences shall be analyzed and approved or permitted under the rules applicable to R-1 zoning district at the time of application or permit request.

1. Building Types and Height

Table 18 identifies the permitted building types and the minimum and maximum heights allowed, by building type within the Urban Village (UV) sector, subject to compliance with all other applicable standards.

Table 18 Building Types and Heights (Stories)

Building Types	Min.	Max.	Max. w/ Bonus
(a) Flexible Block	3	5	8 ⁱ
(b) Flex Building	3	5	6 ⁱ
(c) Rowhouse	2	3	-
(d) Stacked Apartment	2	4	-
(e) Single Family House	-	3	-

Key
 Permitted ●
 Non-Permitted ○
 N/A -

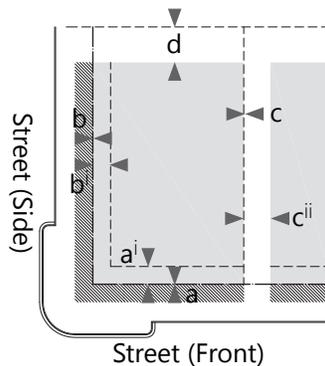
- (i) Unless primary frontage is along Franjo road, buildings adjacent to a Neighborhood Sector (UV) or Neighborhood Village (NV), maximum height, with bonuses, shall be limited to six (6) stories to provide compatibility with the neighborhood scale.

Note 28: 6 stories is not compatible next to a single story fabric. Minimum should be reduced to 2 stories.

2. Building Setbacks

Figure 15 and corresponding Table 19 identifies any required setbacks, for the (UV) sector. Where applicable, the required setbacks shall apply to all stories of a building, at the ground level. Setbacks shall be measured from the build-to line along the street frontages and the property line for all other sides.

Figure 15 Building Setbacks



Key
 Development Area
 Encroachment Area
 Build-to Line
 Property Line

Table 19 Building Setbacks

Required Setbacks	
(a) Street (Front) ⁱ	0/10ft
(b) Street (Side) ⁱ	0/10ft
(c) Side (Interior) ⁱⁱ	0ft min./5ft ⁱⁱⁱ min.
(d) Rear	15ft min.

- (i) Street (Front and Side) setback shall be 10ft where a stoop frontage type is used in association with an applicable building type, with frontage on a Typical Street, in the (UV) Sector. (Franjo Road frontage excluded)
- (ii) Side (Interior) setback shall be minimum 5ft for Stacked Apartment and Single Family House building types, designed as an edge-yard, in the (UV) sector.
- (iii) For zero-lot line development within the (UV) sector, there shall be minimum 5ft side setback at each end of consolidated development parcel

3. Frontage Requirements

All ground floors of building types within the (UV) sector shall comply with the allowed frontage types in Table 20, subject to compliance with all other applicable standards. Refer to Sec.4.04 A-E 7(b) for permitted frontage type by individual building types.

Table 20 Frontage Types

Frontage Types	
(a) Arcade	■
(b) Storefront	■
(c) Forecourt	■
(d) Stoop	■
(e) Porch	□

Key
 Allowed ■
 Non-Allowed □
 N/A -

Why are porches not allowed? There is precedent for townhouses with porches.

4. Encroachments

Figure 16 and corresponding Table 21 identifies the encroachments allowed into the build-to line, and the vertical clearance and horizontal projection by encroachment type, within the (UV) sector.

For buildings with a stoop frontage, encroachment shall be considered anything that projects from the setback line towards the established build-to line.

Figure 16 Encroachments

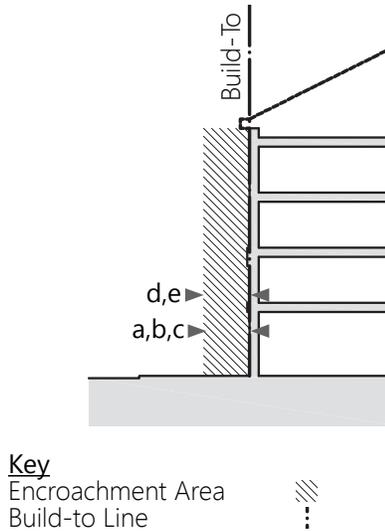


Table 21 Encroachments

Encroachments	Vertical Clearance (Ground)	Horizontal Projection
Street (Front and Side)		
(a) Signage ⁱ	8ft min.	24" max.
(b) Lighting ⁱ	8ft min.	24" max.
(c) Awning	10ft min.	6ft max.
(d) Balcony	12ft min.	6ft max.
(e) Window	12ft min.	4ft max.
Side (Interior)	N/A	N/A
Rear	N/A	N/A

- (i) Horizontal projection of encroachment can exceed the maximum allowable distance at a rate of six (6") inches for every foot above the eight (8) feet min. vertical clearance, for a total of 48" max.
- (ii) Sidewalk cafes and outdoor table service may encroach at the sidewalk level, provided that it shall be in compliance with the Village's Sidewalk Café Ordinance. See Sec. 30-60.17.

5. Parking Access and Setbacks

All off-street parking and associated access that does not occur within a parking structure, in the (UV) sector, shall be developed according to the requirements provided in Figure 17-18 and accompanying Table 22-23.

See previous parking notes

Figure 17 Parking Access and Setbacks

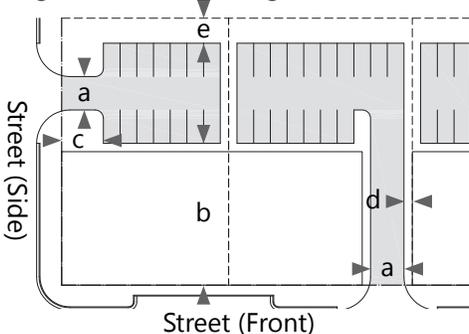


Table 22 Driveway Dimensions

Parking Access (Driveway)	
(a) 1-way	10ft min.
(a) 2-way	20ft min.
Parking	Not Allowed

Figure 18 Shared Access

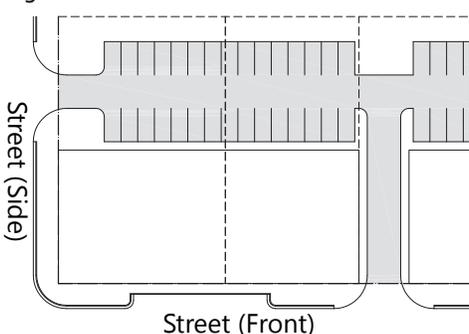


Table 23 Parking Area Setback

Required Setbacks	
(b) Street (Front)	30ft min.
(c) Street (Side)	10ft min.
(d) Side (Interior) ⁱ	5ft min.
(e) Rear ⁱ	5ft min.

- (i) Side (Interior) and Rear setback shall be min. five (5) feet and landscaped with ground cover and a low hedge/wall or fence, where driveway access is provided to the rear of the lot. See Sec.4.03 B 1(ai-ii).

Shared access to parking shall be encouraged to limit the frequency of curb cuts along the primary frontage street, which maintains the improved streetscape and street front at the pedestrian level. See Sec.4.03 B 2. A cross access agreement between property owners shall be provided to the Village of Palmetto Bay depicting shared access.



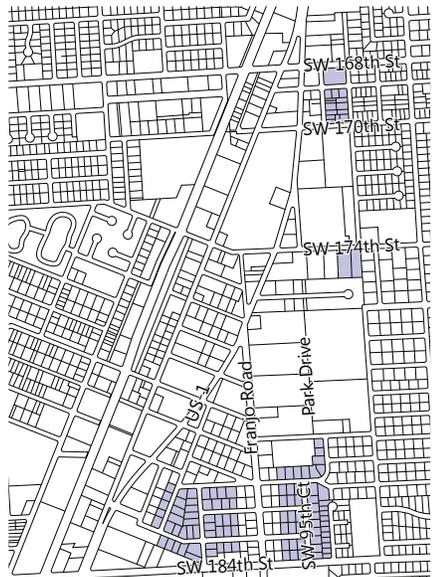
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D. Neighborhood Village (NV)*

Note 31C: This table is inconsistent with Section 1.09.5.A. Eligibility. Bonuses are not eligible in the NV.

Sector Summary

NV Neighborhood Village						
Building Types	Lot Size W x D	Residential Density*	Building Height	Uses by Story		Private Open Space
Flex Building	80'x100'	24 du/ac	3 stories (min.) 5 stories (max.) 6 stories (with bonus)	1st 2nd+	C-R/O/R O/R	15% of site
Row-house	80'- 125'x100'	24 du/ac <i>text</i>	2 stories (min.) 3 stories (max.)	1st 2nd+	C-R/O/R R	400 sq.ft. per unit
Stacked Apartment Building	80'-200'x 100'	24 du/ac	2 stories (min.) 4 stories (max.)	1st 2nd+	R R	10% of site
Single-Family House	45'- 100'x100'	24 du/ac	3 stories (max.)	1st 2nd+	R R	10% of site



*Maximum Base Density: Maximum base density refers to the number of initial residential units permitted per acre before adding available reserve and/or TDR units.

Streets and Building Placement							
Street Type	ROW	Build-To Line		Sidewalk	Bike Lanes	Uses (at Street)	Glazing (at Street)
		Primary	Secondary				
Urban 1 (TS-U1)	50' or 60'	30' (from centerline of road)	45' (from centerline of road)	10'	N/A	C-R O R	70% min. (C-R/O only)
Neighborhood 1 (TS-N1)	50' or 60'	30' (from centerline of road)	45' (from centerline of road)	10'	N/A	R	N/A

Key: Commercial-Retail: C-R Office: O Residential: R

Note 31: Page 14 shows density at 14 du/ac. Table shows it at 24du/ac? Which is it.
 Why is the least intense area proposed at the highest density?
 Why is a flex building allowed "everywhere"?
 Note 31A: Reduce minimum height to 2 stories
 Note 31B: Why is most intense base (4 stories without step back prescribed for what appears to want to be the least intense area. Consider adding a step back and reducing height (3 stories max).



(*) Notwithstanding any provision of this code amendment or the Downtown Urban Village Ordinance, any land, development applications or permit requests regarding properties within the Neighborhood Village or Urban Village, which are currently (as of January 4, 2016, which is the date of passage of this amendment on second reading) constructed as single family residences shall be analyzed and approved or permitted under the rules applicable to R-1 zoning district at the time of application or permit request.

1. Building Types and Height

Table 24 identifies the permitted building types and the minimum and maximum heights allowed, by building type within the Neighborhood Village (NV) sector, subject to compliance with all other applicable standards.

Table 24 Building Types and Heights (Stories)

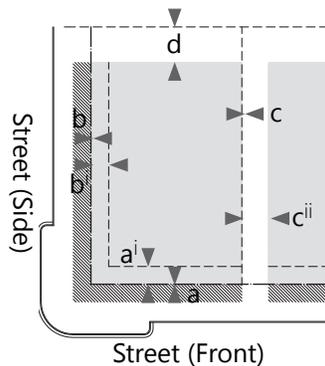
Building Types	Min.	Max.	Max. w/ Bonus
(a) Flexible Block	-	-	-
(b) Flex Building	3	5	6
(c) Rowhouse	2	3	-
(d) Stacked Apartment	2	4	-
(e) Single Family House	-	3	-

Key
 Permitted ●
 Non-Permitted ○
 N/A -

2. Building Setbacks

Figure 19 and corresponding Table 25 identifies any required setbacks, for the (NV) sector. Where applicable, the required setbacks shall apply to all stories of a building, at the ground level. Setbacks shall be measured from the build-to line along street frontages and the property line for all other sides.

Figure 19 Building Setbacks



Key
 Development Area
 Encroachment Area
 Build-to Line
 Property Line

Table 25 Building Setbacks

Required Setbacks	
(a) Street (Front) ⁱ	0/10ft
(b) Street (Side) ⁱ	0/10ft
(c) Side (Interior) ⁱⁱ	0ft min./5ft ⁱⁱⁱ min.
(d) Rear	15ft min.

- (i) Street (Front and Side) setback shall be 10ft where a stoop/porch frontage type is used in association with an applicable building type, with frontage on a Typical Street, in the (NV) Sector. (Franjo Road frontage excluded)
- (ii) Side (Interior) setback shall be minimum 5ft for Stacked Apartment and Single Family House building types, designed as an edge-yard, in the (NV) sector.
- (iii) For zero-lot line development within the (NV)sector, there shall be minimum 5ft side setback at each end of consolidated development parcel

3. Frontage Requirements

All ground floors of building types within the (NV) sector shall comply with the allowed frontage types in Table 26 subject to compliance with all other applicable standards. Refer to Sec.4.04 A-E 7(b) for permitted frontage type by individual building types.

Table 26 Frontage Types

Frontage Types	
(a) Arcade	□
(b) Storefront	■
(c) Forecourt	■
(d) Stoop	■
(e) Porch	■

Key
 Allowed ■
 Non-Allowed □
 N/A -

4. Encroachments

Figure 20 and corresponding Table 27 identifies the encroachments allowed into the build-to line, and the vertical clearance and horizontal projection by encroachment type, within the (NV) sector.

For buildings with a stoop/porch frontage, encroachment shall be considered anything that projects from the setback line towards the established build-to line.

Figure 20 Encroachments

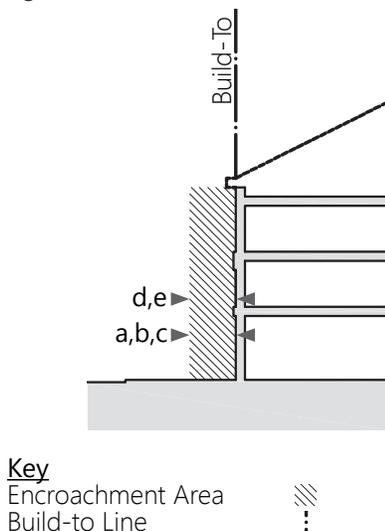


Table 27 Encroachments

Encroachments	Vertical Clearance (Ground)	Horizontal Projection
Street (Front and Side)		
(a) Signage ⁱ	8ft min.	24" max.
(b) Lighting ⁱ	8ft min.	24" max.
(c) Awning	10ft min.	6ft max.
(d) Balcony	12ft min.	6ft max.
(e) Window	12ft min.	4ft max.
Side (Interior)	N/A	N/A
Rear	N/A	N/A

- (i) Horizontal projection of encroachment can exceed the maximum allowable distance at a rate of six (6") inches for every foot above the eight (8) feet min. vertical clearance, for a total of 48" max.
- (ii) Sidewalk cafes and outdoor table service may encroach at the sidewalk level, provided that it shall be in compliance with the Village's Sidewalk Café Ordinance. See Sec. 30-60.17.

5. Parking Access and Setbacks

All off-street parking and associated access that does not occur within a parking structure, in the (NV) sector, shall be developed according to the requirements provided in Figure 21-22 and accompanying Table 28-29.

Note 33: See previous parking notes.

Figure 21 Parking Access and Setbacks

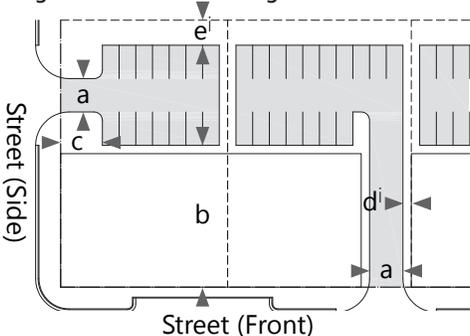


Table 28 Driveway Dimensions

Parking Access (Driveway)	
(a) 1-way	10ft min.
(a) 2-way	20ft min.
Parking	Not Allowed

Figure 22 Shared Access

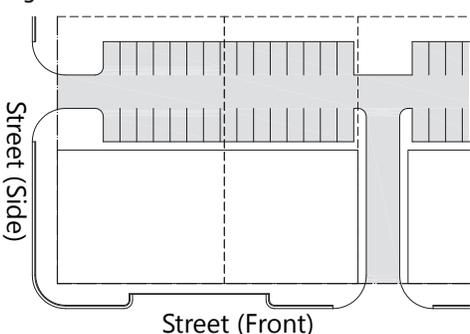


Table 29 Parking Area Setback

Required Setbacks	
(b) Street (Front)	30ft min.
(c) Street (Side)	10ft min.
(d) Side (Interior) ⁱ	5ft min.
(e) Rear ⁱ	5ft min.

- (i) Side (Interior) and Rear setback shall be min. five (5) feet and landscaped with ground cover and a low hedge, where driveway access is provided to the rear of the lot. See. Sec.4.03 B 1(ai-ii).

Shared access to parking shall be encouraged to limit the frequency of curb cuts along the primary frontage street, which maintains the improved streetscape and street front at the pedestrian level. See Sec.4.03 B 2. A cross access agreement between property owners shall be provided to the Village of Palmetto Bay depicting shared access.



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30-50.23.4 Architecture Standards

Section 4.01 Purpose

This section identifies the range of building types permitted within the Village of Palmetto Bay Downtown Urban Village (DUV). The individual building types are presented in a range of most urban to least urban types. Each type is allowed as identified on the summary Table 30, and its requirements are described on the subsequent pages.

Section 4.02 Permitted Building Types

Subject to the requirements of the applicable sector, a proposed building type shall be designed as one of the building types.

- A. Flexible Block** This should be re-categorized as "mixed-use building"
A potential mixed-use building type, occupied by one of or a combination of multi-family residential, commercial or offices at the ground floor and office/multi-family residential units on the floor(s) above. The building is intended to front more than two (2) street frontages and accommodate larger footprint commercial uses or structured parking within the envelope.
- B. Flex Building** This should be re-categorized as "mixed-use building"
A potential mixed-use building type, occupied by one of or a combination of multi-family residential, commercial or offices at the ground floor and office/multi-family residential units on the floor(s) above. The building is intended to front no more than two (2) street frontages and the shallower footprint provides a versatile form for smaller lots with programmatic requirements, like service or parking in the rear.
- C. Rowhouse**
A residential building type that shares a party wall with the structure next to it. Each individual structure is occupied by one residence, on all floors, in an array of at least three (3) structures, side by side along the primary street frontage.
- D. Stacked Apartment** This should be re-categorized as "multifamily residential building"
A neighborhood scaled, multi-family residential building type with similar residential units throughout all floors of the building. Floor plans are intended to accommodate a variety of unit types.
- E. Single Family House**
A residential building type that accommodates one primary residence on all floor(s) of the structure and occupies the totality of the site in and of itself.

Table 30 **Summary of Permitted Building Types**

Building Types	Residential	Commercial/Retail	Office	Lot Width ⁽ⁱ⁾	Lot Depth	Sectors			
						min.-max.	min.-max.	DV	DG
(A) Flexible Block	■	■	■	160ft (min.)	160ft (min.)	●	●	●	○
(B) Flex Building	■	■	■	80ft (min.)	100ft (min.)	●	●	●	●
(C) Rowhouse	■	■	■	80ft-125ft	100ft (min.)	○	○	●	●
(D) Stacked Apartment	■	□	□	80ft-200ft	100ft (min.)	○	○	●	●
(E) Single Family House	■	□	□	45ft-100ft	100ft (min.)	○	○	●	●

- Key**
- Permitted
 - Non-Permitted
 - Allowed
 - Non-Allowed

(i) Measurement taken from the front property line of each lot.

Section 4.03 General Development Parameters

All building types are subject to the following general development parameters:

A. Lot Width and Depth Standards

1. All buildings shall be designed within a specific, individual or assembly of lots.
 - (a) The lot width and depth shall be determined as follows, Figure 23:
 - (i) Front (Lot Width): Primary street frontage
 - (ii) Side (Lot Depth)
 - (iii) Rear (Lot Width)
 - (b) On corner lots fronting two or more streets, the highest ranking street on the Street Hierarchy Plan, Sec.2.05, shall be used to comply with the lot width requirement per building type.
 - (c) On corner lots fronting multiple streets of the same designation on the Street Hierarchy Plan, Sec.2.05, either street frontage may be used to comply with the width/frontage required per building type.

Figure 23 Measuring Lot Dimensions

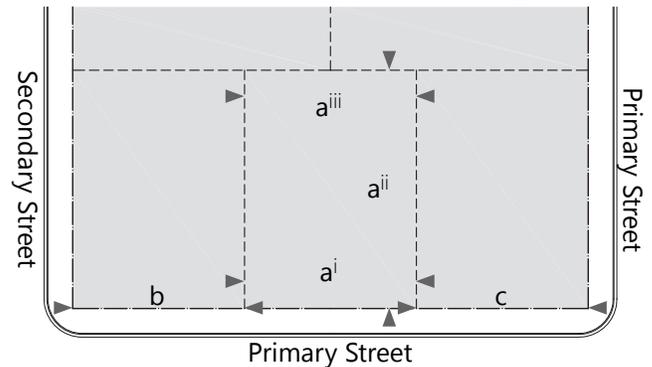


Figure 24 Access via Alley

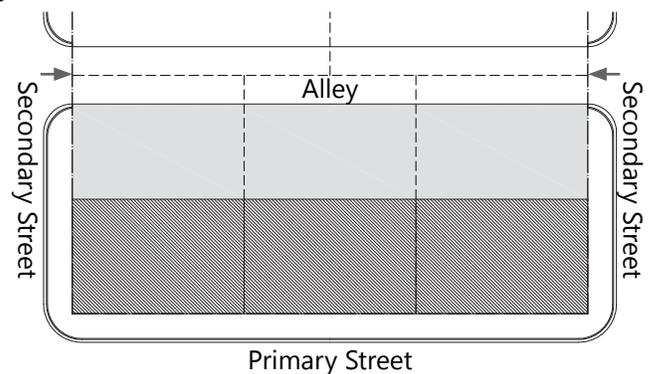


Figure 25 Access without Alley

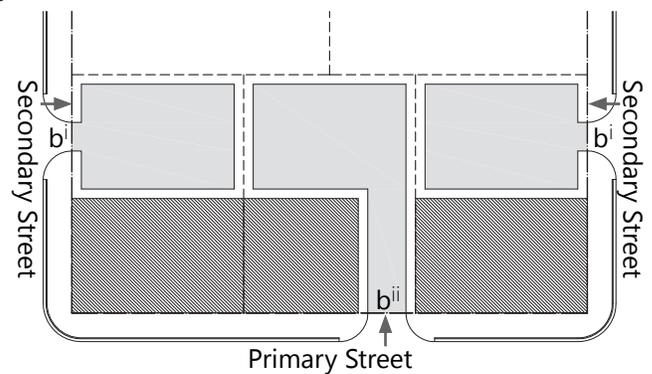
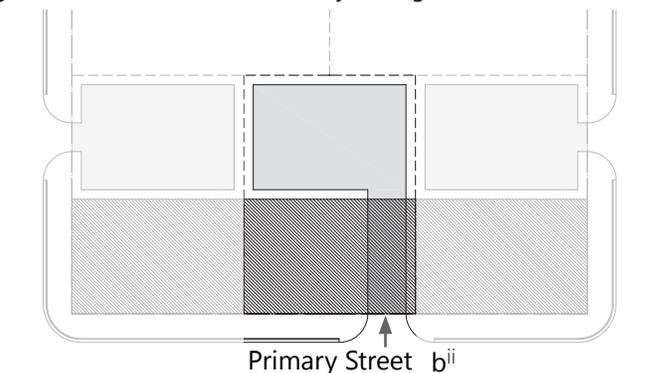


Figure 26 Access without Alley-Through Ground Floor of Building



B. Access Standards

1. All parking and services shall be accessed according to the following:
 - (a) Where there is an alley present:
 - (i) Parking and services shall be accessed from the alley, Figure 24.
 - (ii) All lots, with primary frontage on Franjo Road (SW 97th Avenue), shall be accessed from the lower ranking secondary streets, via shared access driveway/alley.
 - (b) Where there is no alley present:
 - (i) For sites with multiple street frontages, parking and services should be accessed, via driveway, from the lowest ranking street on the Street Hierarchy Plan, Sec.2.05, and Figure 25.
 - (ii) For landlocked sites with singular frontage on a primary street, access to parking and services should be via driveway passage through or driveway alongside the first floor of the building Figure 25-26.
2. Shared access, Figure 27, between adjacent property owners via a cross-access agreement filed with the the Village of Palmetto Bay is encouraged to reduce curb cuts along street frontage and provide consolidated parking areas and inter-block circulation.

C. Parking Standards

1. Parking Standards shall be provided on-site or off-site through a centralized parking system in order to encourage development of property, consolidate parking and implement the vision of the Village of Palmetto

Where is the centralized parking system identified/defined?

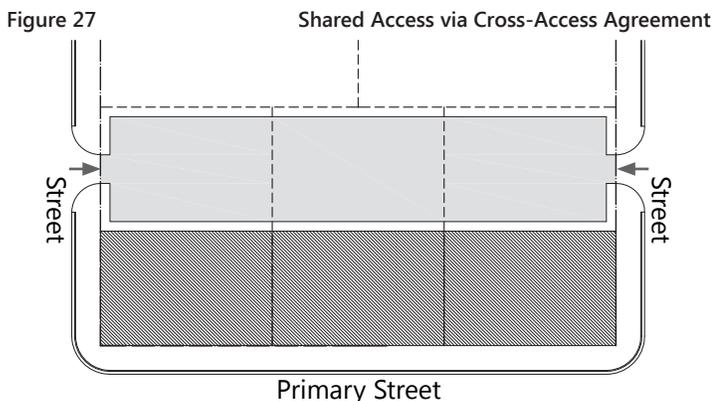
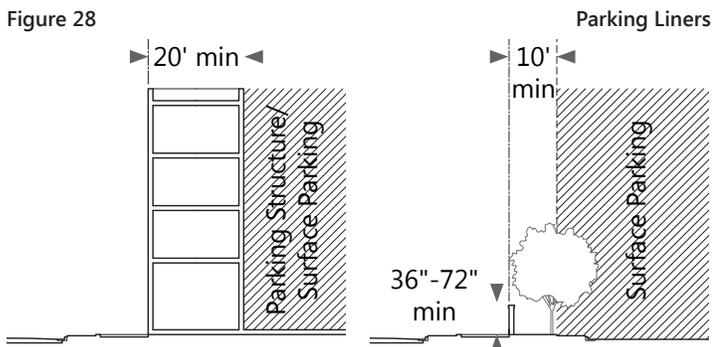


Table 31 Parking by Use

Building Types	Parking Requirement
(A) Single Family Residential <i>Consider parking as an incentive. Address by location.</i>	<ul style="list-style-type: none"> • Single family detached: 2 spaces/unit • Rowhouse: 2 spaces/unit
(B) Multi-Family Residential	<ul style="list-style-type: none"> • Units 750 sq. ft. and less 1 space/residential unit • Units more than 750 sq. ft. 1.5 spaces/residential unit
(C) Housing for the Elderly	• 0.5 spaces/unit
(D) Hotel/Motel	<ul style="list-style-type: none"> • 1 space/guest rooms (up to 40 guest rooms) • 0.5 spaces/guest room (after 40)
(E) Retail	• 1 space/300 sq. ft. of gross floor area
(F) Offices/Health Care <i>Office is not the same as HC</i>	• 1 space/400 sq. ft. of gross floor area
(G) Food and Drink Establishments	• 1 space/50 sq. ft. of patron area

All other uses shall comply with the parking standards provided in 30-70.8 of the Village of Palmetto Bay Code.



If garages need to be lined, why does it matter which street they face?

Bay Downtown Urban Village (DUV). See Sec.4.04 A-E 3(b-c) for parking options specific to each building type.

- (a) On-site parking shall be considered any parking structure, surface parking, tuck under parking, private parking garage or surface parking pad within the property lines and applicable build-to lines on private property.
 - (i) The roof of all parking structures shall be programmed with usable building surface such as: green roof, amenity deck (private open space) or for renewable energy generation.
 - (b) Off-site parking shall be considered any parking structure, surface parking or on-street parking located on a development parcel or adjacent public right of way other than the parcel being developed.
 - (i) The roof of all parking structures shall be programmed with usable building surface such as: green roof, amenity deck (private open space) or for renewable energy generation.
2. Parking requirements may be satisfied off-site within a parking structure or surface parking lot that shall be within 1,000 feet of the nearest point of the parcel being developed.
 - (a) For all off-site parking in a parking structure or surface parking lot subject to the standards above, applicant/owner must submit a parking covenant attached to proposed development plans.
 3. At a minimum, the number of parking spaces shall be provided in accordance with Table 31
 - (a) Reductions from the total parking spaces required by the development are offered as part of the Village Parking Incentives Program, Sec.1.07 A.
 - (b) In addition to the vehicle parking, for every 10 parking spaces required, 1 bicycle parking space shall be provided.
 - (i) A minimum of 25% of the required bicycle parking shall be provided along the primary street frontage as identified in the Street Hierarchy Plan, Sec.2.05.
 4. Parking structures shall be lined with a minimum of 20' of habitable building space along streets, to preserve the character of the street facade, Figure 28.
 5. Surface Parking shall be lined with habitable building space or decorative wall/landscaping at the build-to line, Figure 28. **Are walls permitted along A streets?**
 - (a) Wall/landscape hedge shall be minimum 36 inches and maximum 72 inches.
 - (b) Surface parking shall not encroach into any required yards.
 6. Private parking garages shall be located at the rear of the lot or facing the side of the lot. For lots with multiple frontages the garage shall face the lowest ranking street. See Sec.2.05 for Street Hierarchy Plan.
 7. Mechanized parking shall be allowed towards parking

counts for all off-street parking within the Downtown Urban Village (DUV) and shall comply with the following:

- (a) A queuing analysis is submitted with application for review. See Sec.1.05
 - (b) Mechanized parking spaces shall not account for more than 50% of the total parking count.
 - (i) For residential developments on lots less than 15,000 sq.ft. 100% of the parking requirements may be mechanical parking.
8. Parking for individuals with disabilities shall comply with the standards of the Florida Building Code.

D. Services Standards

- 1. All services shall be subject to the following:
 - (a) Where there is an alley present:
 - (i) All services, including utility access, above ground equipment and trash enclosures shall be located on alleys.
 - (b) Where there is no alley present:
 - (i) All services, including utility access, above ground equipment and trash enclosures shall be located within the build to line, and subject to all applicable standards for building placement.
 - (c) All services shall be screened from the street view by habitable building space or landscaping/wall and shall not encroach into required setback and landscaped areas.
- 2. All new utilities, other than fire hydrants shall run underground and be accessed according to the standards of this section.
 - (a) For all development with primary frontage along SW 97th Avenue (Franjo Road), all existing street utilities must be replaced underground at the time of development.

E. Private Open Space Standards

- 1. Private open space shall be count in the form of courtyards, balconies, terraces, lawns, community gardens, amenity recreation decks and landscaped roof terraces/gardens on buildings/parking structures.
 - (a) Permitted frontage types shall count towards meeting the private open space requirements of these regulations. See Sec.4.04.
 - (b) The area of any covered patio, gazebo or other roofed shade structures shall count towards meeting the private open space requirements, as long as two (2) sides are opened to the outside.

Private balconies count towards private open space requirements? Porches and stoops count?

F. Landscape Standards

- 1. Except as provided herein, landscape shall be provided as required in Village of Palmetto Bay Landscape Regulations, Sec. 30-100.1
- 2. Street trees shall be planted at a maximum of 25ft

Figure 29

Tree Grates

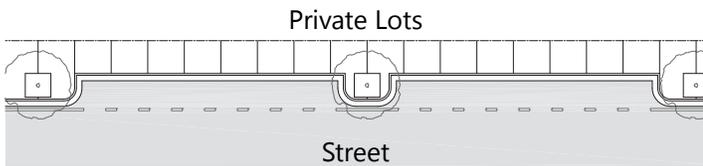


Figure 30

Landscape Islands

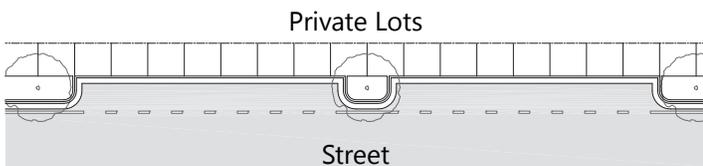
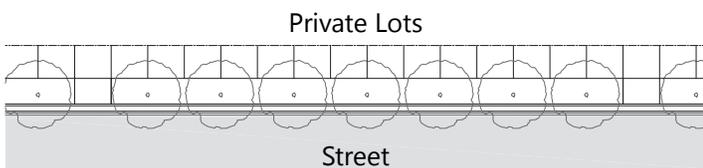


Figure 31

Continuous Landscape Strips



Note 39: Frontage standards should vary according to street type.

Sector	Minimum Frontage Occupation
Sector	Frontage Occupation (min.)
(DV) Downtown Village	80%
(DG) Downtown General	80%
(UV) Urban Village	70%
(NV) Neighborhood Village	60%

average on center, unless integrated with parking, with a minimum caliper of five (5in.) inches

- (a) As indicated in the Street Connectivity Standards, Sec.5, street trees shall be planted in one or more of the following methods:
 - (i) Tree Grates: Trees are planted within openings on the sidewalk, between groups of parallel parking spaces. Openings shall be covered by permanently installed grates perforated to permit natural irrigation, which are flush to the sidewalk, Figure 29.
 - (ii) Landscape Islands: Trees are planted in the landscaped area between groups of parallel parking spaces. Area should be covered with grass and other natural ground cover to permit natural irrigation Figure 30.
 - (iii) Continuous Landscape Strips: Trees are planted in the area between the curb or roadway edge and the sidewalk. This area, in addition to the required trees, shall be covered with grass and other natural ground cover to permit natural irrigation Figure 31.

G. Frontage Standards

1. Buildings shall occupy a percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
 - (a) Lots within the (UV) sector and with primary frontage along Franjo Road shall occupy 80% minimum at the build-to line.
 - (b) For buildings with multiple street frontages, percentage must be applied to the highest ranking street according to Sec.2.05 Street Hierarchy Plan.
2. All buildings shall have a street level frontage types, according to Sec.4.04 A-E 7(b) and shall comply with the applicable standards in Sec. 4.05.
 - (a) For the purpose of calculating the percentage of frontage occupied at the build-to line, permitted frontage types shall count towards the minimum requirement, as long as, all other standards are met.
3. For buildings on sites with greater than 300 feet of frontage, along a street, a pedestrian only paseo, minimum 15 feet in width shall be provided.
 - (a) Pedestrian paseo shall be designed on the frontage so that cross-block access is no more than 200 feet from a Street or other pedestrian paseo.

H. Building Size and Massing

1. Buildings shall be constructed as variable masses, with applied horizontal and vertical extrusions to create the desired building form.
 - (a) No building shall occupy more than 250 feet of continuous frontage, along any street within the

- DUV.
 - (b) For building articulation, a break in building facade shall occur every 60 feet max. on buildings that occupy 150 feet, or more, of continuous frontage.
 - (i) Break in building facade shall be recessed from the build-to line, up to two (2) feet maximum and shall be at a minimum, be the height of the base element of the building, where required.
- 2. Building height shall not exceed 125 feet anywhere within the Downtown Urban Village (DUV).
 - (a) Parapet wall shall be a maximum 40 inches tall, measured from the top of the highest slab for a flat roof.
 - (b) Any objects/structures, such as for mechanical equipment or recreational use shall not encroach into the highlighted area in Figure 32, and shall not exceed maximum 15 feet in height, measured from the top of the parapet wall.
 - (i) Stair and elevator towers shall be exempt from the highlighted area Figure 32, provided that they be a design element, consistent with the architectural concept of the building.
- 3. Buildings shall be built according to the minimum/maximum heights by sector, Sec.3.02 A-D 1. For the purpose of calculating the number of stories in a building, stories shall be defined as the occupied space between finished floor and finished ceiling. Table 33 identifies the permitted heights for individual stories, within each building type.
 - (a) Basements shall not be considered towards the building height (stories), when the finished surface of the floor, one story above the basement is less than four (4ft) feet above grade.
 - (b) Above ground structure that occupy any level shall be considered towards the building height (feet).
 - (c) Mezzanine shall not count towards the number of floors provided that the total area of mezzanine level is less than 40% of the floor area of that story.
- 4. Multi-family residential buildings shall meet the density requirements, as identified in the Residential Density Map.
- 5. Residential components of a multi-family dwelling units, Figure 33, shall be any combination of the following dwelling units:
 - (a) Flat: a single (1) story dwelling unit, occupied by one (1) household
 - (b) Loft: a double-story height dwelling unit with or without mezzanine, occupied by one (1) household
 - (c) Townhouse: a two (2) or more story dwelling unit, occupied by one (1) household.
- 6. All residential units shall be 24 inches minimum above flood level criteria or average crown of the road, whichever is greater.

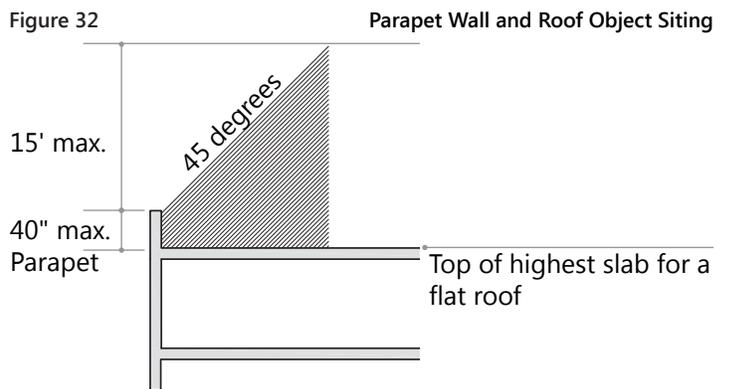


Table 33 Permitted Heights by Story

Building Types	Ground Story	Story 2+
Flexible Block Flex Building	14ft (min.)-18ft (max.)	9ft (min.)-12ft (max.)
Rowhouse Stacked Apartment	10ft (min.)-14ft (max.)	8ft (min.)-12ft (max.)
Single Family House	9ft. (min.)-12ft (max.)	

Key
 Permitted ●
 Non-Permitted ○

Note 40: Not sure why there is a provision for a basement. However for a story not to be considered it should be no more than 3' above grade if use above it is office or residential. In the case of retail, underground story should not be above grade.

I. Accessory Structures

Figure 33

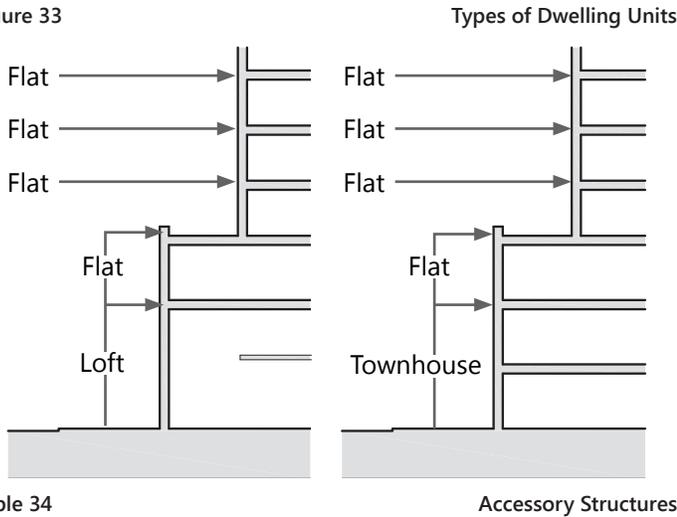
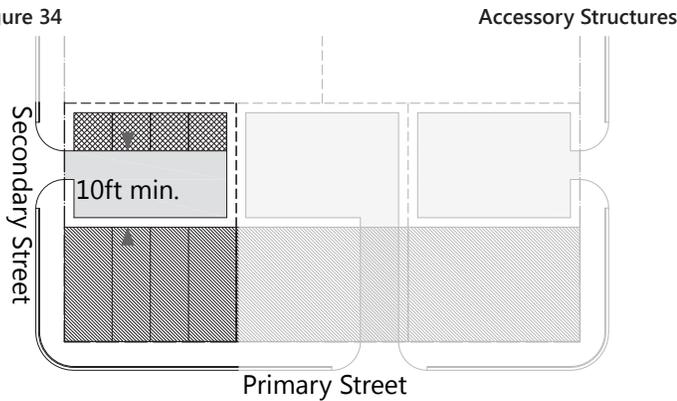


Table 34

UV NV	Building Types	Accessory Structure	Height (max.)
	Flexible Block	○	N/A
	Flex Building	○	N/A
	Rowhouse	●	2 stories
	Stacked Apartment	●	2 stories
	Single Family House	●	2 stories

Key
 Permitted ●
 Non-Permitted ○

Figure 34



Key
 Principal Building ▨
 Accessory Structure ▩

- Accessory structures shall be permitted with specifically allowed building types Table 34, within the Urban Village (UV) and the Neighborhood Village (NV) sectors.
- Accessory structures shall be subject to all required setbacks of the sector, Sec.3.02 A-D 1, and must be 10ft minimum from the principal building, Figure 34.
- The height of an accessory structure shall not exceed the minimum allowed height of the principal building on site.

J. Accessory Dwellings

- Accessory Dwellings shall be permitted within the accessory structures and specifically allowed building types, Sec.4.04 C,E 10(a), within the Urban Village (UV) and the Neighborhood Village (NV) sectors.
- The height of an accessory dwelling shall be one (1) story in height and shall not exceed 600 sq.ft. area.
 - A flat, Sec.4.03 H 5(a), may be located on the second floor of an accessory structure when first floor is occupied by a private garage.
 - Accessory dwellings shall not have a culinary facility within unit.

K. Lighting

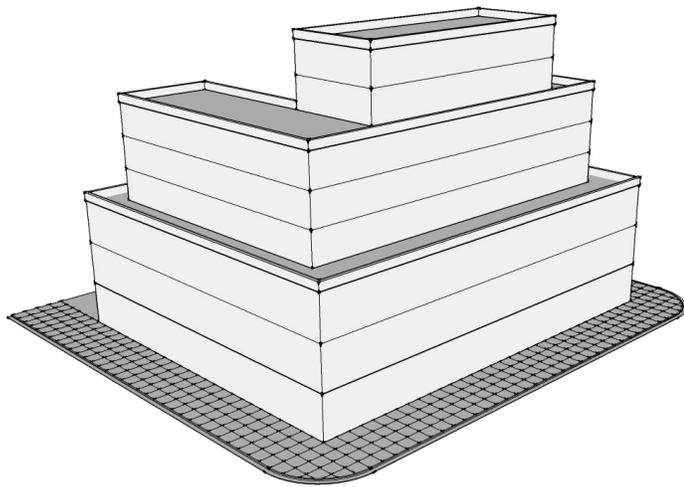
- All Lighting shall comply with the following:
 - Lighting shall be provided in these areas: drive-ways and parking areas, sidewalks and pedestrian paseos, commercial establishments, entryways, recreation areas and multi-family common areas and entryways.
 - lighting of these area shall comply with Sec. 30-60.6.-Lighting of the Village of Palmetto Bay Code.
 - All light fixtures shall be of a pedestrian scale, with a maximum height of 18 feet and a maximum spacing between fixtures of 60 feet.
- The type and style of light fixtures shall be approved by the Director, based on uniformity of types, location, right-of-way width along streets and illumination and light trespass.
 - Light standards shall meet and maintain the recommended luminance range and uniformity for each use and/or structure, as specified in the latest issue of the Illuminating Engineering Society of North America's (IESNA) publication.
 - Light standards shall meet and maintain the recommended luminance range to minimize light trespass, as specified in the latest issue of the Illuminating Engineering Society of North America's (IESNA) publication.

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Section 4.04 Specific Development Parameters by Building Type

Figure 35

Flexible Block Massing



Graphic shows a decrease in floor plate as building increases in height. Is this regulated?

Figure 36

Minimum Lot Size

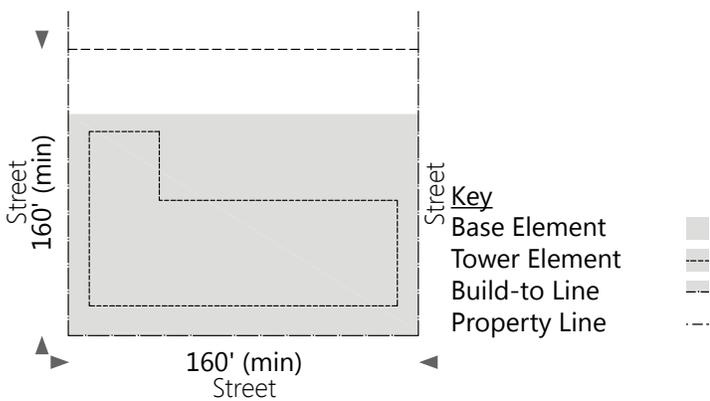


Table 35

On-Site and Off-Site Parking Options

On- Street Parking	
Parking Structure	■
Surface Parking	■
Tuck Under Parking	■
Parking Garage (Private)	□
Surface Parking Pad	□
Off- Site Parking	
Parking Structure	■
Surface Parking	■
On-Street Parking	■

Key
 Allowed
 Non-Allowed

A. Flexible Block Building

A mixed-use building type, that may accommodate some commercial uses at the ground floor and office/multi-family residential units on the floor(s) above. The building is intended to front more than two (2) street frontages and accommodate larger footprint commercial uses or structured parking within the envelope.

1. Lot Width and Depth
 - (a) All flexible block building types shall be in accordance with Sec.4.03 A in addition to the following:
 - (b) The minimum lot width to accommodate a flexible block building along the primary street frontage shall be 160 feet and the minimum lot depth shall be 160 feet.
2. Access Standards
 - (a) All flexible block buildings shall be accessed in accordance with Sec.4.03 B, in addition to the following:
 - (b) The primary entrance to each unit at the street level shall face and be entered from the highest ranking street. See Sec.2.05 for Street Hierarchy Plan.
 - (c) The primary entrance to the units on floor(s) above shall be accessed through a ground level lobby/courtyard with stairs/elevator connected to a system of corridors leading to the units.
 - (d) Each level of building shall have access to a garage (if applicable) via stairs/elevator.
3. Parking Standards
 - (a) All parking for flexible block buildings shall be in accordance with Sec.4.03 C, in addition to the following:
 - (b) On-Site parking shall be accommodated via parking structure, surface parking, tuck under parking or a combination thereof Table 35.
 - (c) Off-site parking may be accommodated via parking structure, surface parking and on-street parking or a combination thereof Table 35.
4. Services Standards
 - (a) All services for flexible block buildings shall be in accordance with Sec.4.03 D, in addition to the following:
 - (b) Services shall be located out of view of the street and shall not impact the general aesthetic of the architecture of the building.
 - (c) For buildings with street frontages on multiple sides, services should be located to the rear of the lot or screened on the lowest ranking street with an architectural wall, solid fence or landscaped hedge. See Sec.2.05 for Street Hierarchy Plan.
5. Private Open Space Standards
 - (a) All flexible block buildings shall meet the private open space standards set forth in Sec.4.03 E, in addition to the following:
 - (b) All multi-family residential, non-residential and

A. Flexible Block Building continued...

mixed-use developments, with a residential component shall provide a minimum of 15% of the site for common, private open space.

6. Landscape Standards
 - (a) All flexible block buildings shall meet the landscape standards set forth in Sec.4.03 F, in addition to the following:
 - (b) Within the areas designated private open space, tree requirements for the flexible block building typology shall be minimum 20 trees per acre of lot area.
 - (i) a maximum of 35% of the total tree requirement within the private open space on a lot may be met by a payment-in-lieu under the Village Landscape and Open Space Improvement Program. See Sec.1.07 B.
7. Frontage Standards
 - (a) All frontage for flexible block buildings shall be in accordance with Sec.4.03 G, in addition to the following:
 - (b) All flexible block buildings shall have a street level frontage type allowed by sector, according to Table 36.
 - (c) For non-residential use, all habitable, semi public operating spaces at the ground level shall enter from and front the street.
 - (i). all service room, storage closets and private office space shall be located backing corridors or in the rear of the unit, away from the street.
 - (d) For residential uses, only habitable building space shall be oriented towards the street, at the ground level.
8. Building Size and Massing
 - (a) All flexible block buildings shall be in accordance with Sec.4.03 H, in addition to the following:
 - (b) Buildings shall be composed of a base element with secondary tower element(s) of reduced footprint above the base, Figure 37.
 - (i) Each building shall meet requirements for each sector as provided in Sec.3.02 A-D
 - (c) The base element shall occupy a minimum percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
 - (i) Balconies on the facade of secondary massing element shall make up no more than 70% of elevation at that story.
 - (d) Above the third story, building mass should step back 15 feet along the primary frontage and 10 feet along all other sides, where applicable.
 - (e) Each building shall be composed of floors programmed with the uses identified in Figure 38.
 - (i). All ground floor units along Franjo Road shall be designed to accommodate commercial/retail or office uses.
 - (ii) All individual commercial/retail units shall be

Table 36 Permitted Frontage Types by Buildings

Flexible Block	DV	DG	UV	NV
Arcade	●	●	●	-
Storefront	●	●	●	-
Forecourt	●	●	●	-
Stoop	○	○	●	-
Porch	○	○	○	-

Key
 Permitted ●
 Non-Permitted ○
 N/A -

Figure 37 Massing Elements

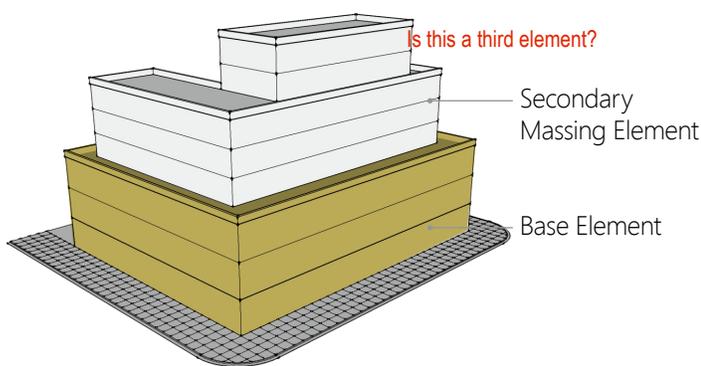
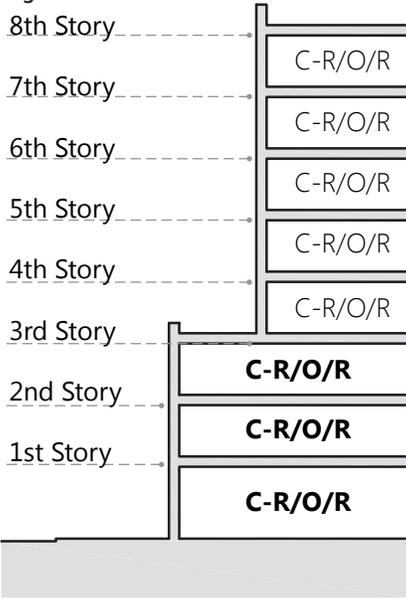


Figure 38



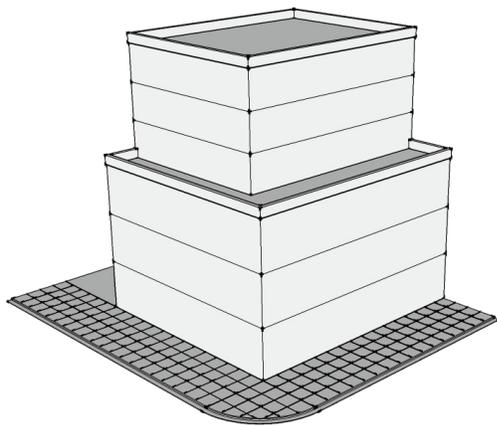
A. Flexible Block Building continued...

- limited to 12,500 sq.ft. maximum area.
- (iii) Buildings with multiple street frontages shall provide fenestration on all sides facing the street.
- (iv) A minimum of 30% of the total building facade shall be fenestrated with windows along all street frontages.
- f. Each unit within the building, regardless of use shall have outdoor exposure and access to open space.
- 9. Accessory Structures
 - (a) Accessory structures shall not be permitted.
- 10. Accessory Dwellings
 - (a) Accessory dwellings shall not be permitted.
- 11. Lighting Standards
 - (a) All flexible block buildings shall be in accordance with Sec.4.03 K.

Consider amending minimum stories to 2.
 Consider indicating ground-floor use specific to street/location.

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Figure 39



Flex Building Massing

B. Flex Building

A mixed-use building type that may accommodate commercial uses at the ground floor and office/multi-family residential units, of similar configuration on the floor(s) above.. The building is intended to front no more than two (2) street frontages and the shallower footprint provides a versatile form for smaller lots with programmatic requirements, like service or parking in the rear.

1. Lot Width and Depth
 - (a) All flex building types shall be in accordance with Sec.4.03 A, in addition to the following:
 - (b) The minimum lot width to accommodate a flex building along the primary street frontage shall be 80 feet and the minimum lot depth shall be 100 feet.
2. Access Standards
 - (a) All flex buildings shall be accessed in accordance with Sec.4.03 B, in addition to the following:
 - (b) The primary entrance to each unit at the street level shall face and be entered from the highest ranking street. See Sec.2.05 for Street Hierarchy Plan.
 - (c) The primary entrance to the units on floor(s) above shall be accessed through a ground level lobby/courtyard with stairs/elevator connected to a system of corridors leading to the units.
 - (d) Each level of building shall have access to a garage (if applicable) via stairs/elevator.
3. Parking Standards
 - (a) All parking for flex buildings shall be in accordance with Sec.4.03 C, in addition to the following:
 - (b) On-Site parking shall be accommodated via parking structure, surface parking, tuck under parking or a combination thereof Table 37.
 - (c) Off-site parking may be accommodated via parking structure, surface parking and on-street parking or a combination thereof Table 37.
4. Services Standards
 - (a) All services for flex buildings shall be in accordance with Sec.4.03 D, in addition to the following:
 - (b) Services shall be located out of view of the street and shall not impact the general aesthetic of the architecture of the building.
 - (c) For buildings with street frontages on multiple sides, services should be located to the rear of the lot or screened on the lowest ranking street with an architectural wall, solid fence or landscaped hedge, minimum 36 inches to 60 inches maximum. See Sec.2.05 for Street Hierarchy Plan.
5. Private Open Space Standards
 - (a) All flex buildings shall be meet the private open space standards set forth in Sec.4.03 E, in addition to the following:
 - (b) All multi-family residential, non-residential and mixed-use developments, with a residential com-

Figure 40

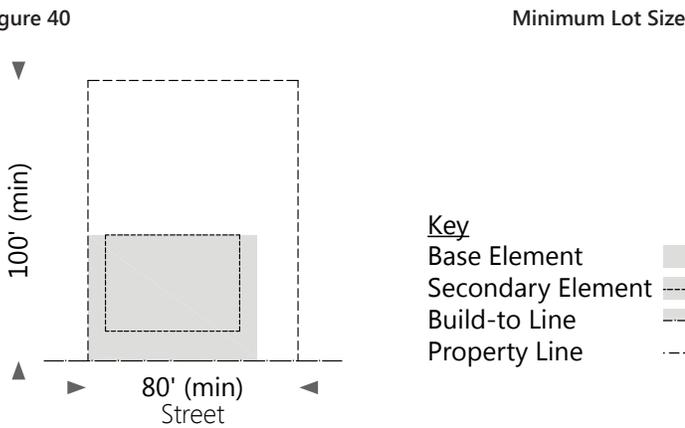


Table 37

On-Site and Off-Site Parking Options

On- Site Parking		Key
Parking Structure	■	
Surface Parking	■	
Tuck Under Parking	■	Non-Allowed
Parking Garage (Private)	□	
Surface Parking Pad	□	
Off- Site Parking		
Parking Structure	■	
Surface Parking	■	
On-Street Parking	■	

B. Flex Building continued...

ponent shall provide a minimum of 10% of the site for common, private open space.

6. Landscape Standards
 - (a) All flex buildings shall meet the landscape standards set forth in Sec.4.03 F, in addition to the following:
 - (b) Within the areas designated private open space, tree requirements for the flex building typology shall be minimum 20 trees per acre of lot area.
 - (i) a maximum of 35% of the total tree requirement within the private open space on a lot may be met by a payment-in-lieu under the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Space Program.
7. Frontage Standards
 - (a) All frontage for flex buildings shall be in accordance with Sec.4.03 G, in addition to the following:
 - (b) All flex buildings shall have a street level frontage type allowed by sector, according to Table 38.
 - (c) For non-residential use, all habitable, semi public operating spaces at the ground level shall enter from and front the street.
 - (i) all service room, storage closets and private office space shall be located backing corridors or in the rear of the unit, away from the street.
 - (d) For residential uses, only habitable building space shall be oriented towards the street, at the ground level.
8. Building Size and Massing
 - (a) All flex buildings shall be in accordance with Sec.4.03 H, in addition to the following:
 - (b) Buildings shall be composed of a base element with secondary massing element(s) of reduced footprint above the base.
 - (i) Each building shall meet requirements for each sector as provided in Sec.3.02 A-D.
 - (c) The base element shall occupy a minimum percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
 - (i) Balconies on the facade of secondary massing element shall make up no more than 70% of elevation at that story.
 - (d) Above the third story, building mass should step back 15 feet along the primary frontage and 10 feet along all other sides, where applicable.
 - (e) Each building shall be composed of floors programmed with the uses identified in Figure 42.
 - (i) All ground floor units along Franjo Road shall be designed to accommodate commercial/retail or office uses.
 - (ii) All individual commercial/retail units shall be limited to 12,500 sq.ft. maximum area.
 - (iii) Buildings with multiple street frontages shall provide fenestration on all sides facing the street.

Table 38 Permitted Frontage Types by Buildings

Flex Building	DV	DG	UV	NV
Arcade	●	●	●	○
Storefront	●	●	●	●
Forecourt	●	●	●	●
Stoop	○	○	●	●
Porch	○	○	○	○

Key
 Permitted ●
 Non-Permitted ○
 N/A -

Figure 41 Massing Elements

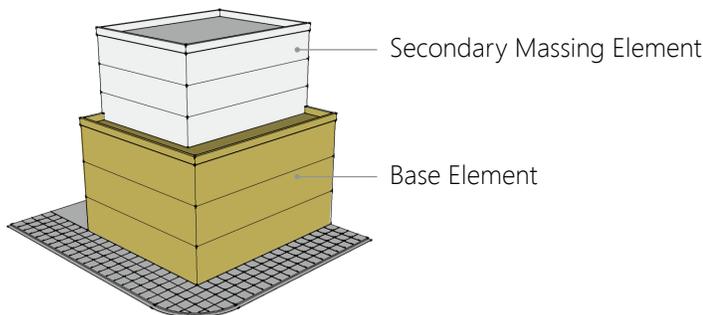
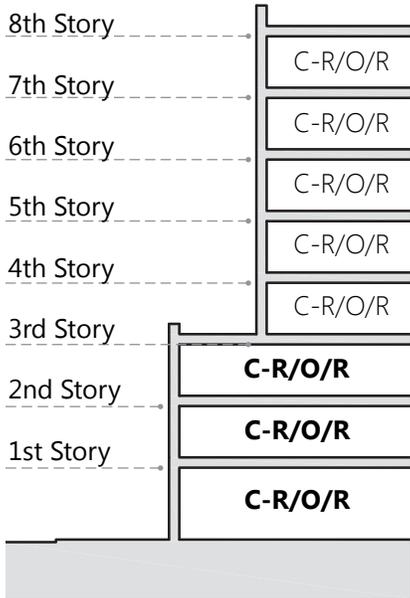


Figure 42



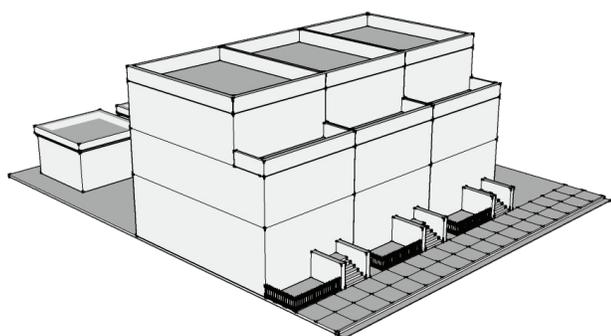
Permitted Uses by Floor B. Flex Building continued...

- (iv) A minimum of 30% of the total building facade shall be fenestrated with windows along all street frontages.
- f. Each unit within the building, regardless of use shall have outdoor exposure and access to open space.
- 9. Accessory Structures
 - (a) Accessory structures shall not be permitted.
- 10. Accessory Dwellings
 - (a) Accessory dwellings shall not be permitted.
- 11. Lighting Standards
 - (a) All flex buildings shall be in accordance with Sec.4.03 K.

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Figure 43

Rowhouse Massing



C. Rowhouse Building

A residential building type that shares a party wall with the structure next to it. Each individual structure is occupied by one residence, on all floors, in an array of at least three (3) structures, side by side along the primary street frontage.

1. Lot Width and Depth
 - (a) All rowhouse building types shall be in accordance with Sec.4.03 A, in addition to the following:
 - (b) The minimum lot width to accommodate 3 rowhouse units side by side along the primary street frontage shall be 80 feet and the maximum lot width shall be 125 feet. The lot depth shall be 100 feet minimum.
2. Access Standards
 - (a) All rowhouse buildings shall be accessed in accordance with Sec.4.03 B, in addition to the following:
 - (b) The primary entrance to each, individual rowhouse unit shall face and be entered from the highest ranking street. See Sec.2.05 for Street Hierarchy Plan.
3. Parking Standards
 - (a) All parking for rowhouse buildings shall be in accordance with Sec.4.03 C, in addition to the following:
 - (b) On-Site parking shall be accommodated via surface parking, tuck under parking, private parking garage or a combination thereof Table 39.
 - (c) Off-site parking may be accommodated via surface parking, on-street parking or a combination thereof Table 39.
4. Services Standards
 - (a) All services for rowhouse buildings shall be in accordance with Sec.4.03 D, in addition to the following:
 - (b) Services shall be located out of view of the street and shall not impact the general aesthetic of the architecture of the building.
 - (c) For buildings with street frontages on multiple sides, services should be located to the rear of the lot or screened on the lowest ranking street with an architectural wall, solid fence or landscaped hedge. See Sec.2.05 for Street Hierarchy Plan.
5. Private Open Space Standards
 - (a) All rowhouse buildings shall be meet the private open space standards set forth in Sec.4.03 E, in addition to the following:
 - (b) All residential, rowhouse development shall provide a minimum of 400 sq. ft. of private open space, per unit.
6. Landscape Standards
 - (a) All rowhouse buildings shall meet the landscape standards set forth in Sec.4.03 F, in addition to the following:
 - (b) Within the areas designated private open space, tree requirements for the rowhouse building

Figure 44

Minimum Lot Size

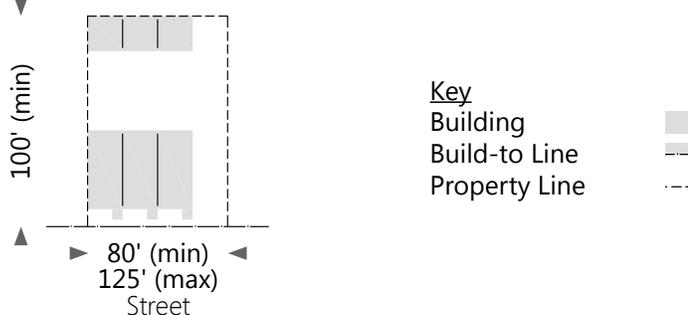


Table 39

On-Site and Off-Site Parking Options

On- Site Parking		
Parking Structure	<input type="checkbox"/>	Key Allowed <input type="checkbox"/> <input checked="" type="checkbox"/> Non-Allowed <input checked="" type="checkbox"/> <input type="checkbox"/>
Surface Parking	<input checked="" type="checkbox"/>	
Tuck Under Parking	<input checked="" type="checkbox"/>	
Parking Garage (Private)	<input checked="" type="checkbox"/>	
Surface Parking Pad	<input type="checkbox"/>	
Off- Site Parking		
Parking Structure	<input type="checkbox"/>	
Surface Parking	<input checked="" type="checkbox"/>	
On-Street Parking	<input checked="" type="checkbox"/>	

C. Rowhouse Building continued...

typology shall be minimum 20 trees per acre of lot area.

- (i) a maximum of 35% of the total tree requirement within the private open space on a lot may be met by a payment-in-lieu under the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Space Program.

7. Frontage Standards

- (a) All frontage for rowhouse buildings shall be in accordance with Sec.4.03 G, in addition to the following:
- (b) All rowhouse buildings shall have a street level frontage type allowed by sector, according to Table 41
- (c) For non-residential use, all habitable, semi public operating spaces at the ground level shall enter from and front the street.
 - (i) all service room, storage closets and private office space shall be located backing corridors or in the rear of the unit, away from the street.
- (d) For residential uses, only habitable, building space shall be oriented towards the street, at the ground level.

8. Building Size and Massing

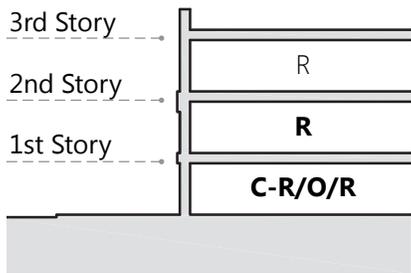
- (a) All rowhouse buildings shall be in accordance with Sec.4.03 H, in addition to the following:
- (b) Buildings shall be composed of a two (2) or three (3) story massing element(s) that meets the requirements for each sector as provided in Sec.3.02 A-D
- (c) The minimum unit frontage shall be 20 feet.
- (d) The building, no less than three (3) rowhouse units, shall occupy a minimum percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
- (e) Between the second and third story, buildings shall provide unique architectural detailing/fenestration that unifies all buildings, horizontally, on a lot by lot basis, regardless of building type, at the street elevation.
- f. Each building shall be composed of floors programmed with the uses identified in Figure 45.
 - (i) All individual commercial/retail units shall be limited to 12,500 sq.ft. maximum area.
 - (ii) Buildings with multiple street frontages shall provide fenestration on all street fronting sides.
 - (iii) A minimum of 30% of the total building facade shall be fenestrated with windows along all street frontages.
- g. For three (3) story rowhouse units only, ground floor may be a separate flat unit type, accessed separately from the primary frontage, with a townhouse dwelling on the second and third floor, accessed by separate stair and front door.

Table 40 Permitted Frontage Types by Buildings

Rowhouse	DV	DG	UV	NV
Arcade	-	-	○	○
Storefront	-	-	●	●
Forecourt	-	-	○	○
Stoop	-	-	●	●
Porch	-	-	○	○

Key
 Permitted ●
 Non-Permitted ○
 N/A -

Figure 45



Permitted Uses by Floor

Key	
Commercial-Retail	C-R
Office	O
Residential	R
Minimum Stories	
Optional Stories	

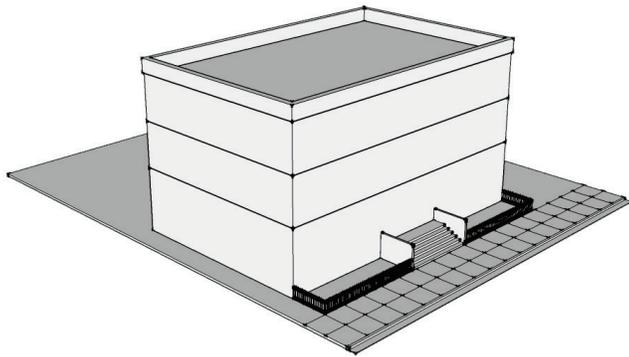
C. Rowhouse Building continued...

- (h) Each unit within the building group, regardless of use shall have outdoor exposure and access to open space.
- 9. Accessory Structures
 - (a) Accessory structures shall be permitted in accordance with Sec.4.03 I.
- 10. Accessory Dwellings
 - (a) Accessory dwellings shall be permitted in accordance with Sec.4.03 J.
- 11. Lighting Standards
 - (a) All rowhouse buildings shall be in accordance with Sec.4.03 K.

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Figure 46

Stacked Apartment Massing



D. Stacked Apartment Building

A multi-family residential building type with similar residential units throughout all floors of the building. Floor plans are intended to accommodate a variety of unit types.

1. Lot Width and Depth
 - (a) All stacked apartment building types shall be in accordance with Sec.4.03 A, in addition to the following:
 - (b) The minimum lot width to accommodate a stacked apartment building along the primary street frontage shall be 80 feet and the maximum lot width shall be 200 feet. The lot depth shall be 100 feet minimum.
2. Access Standards
 - (a) All stacked apartment buildings shall be accessed in accordance with Sec.4.03 B, in addition to the following:
 - (b) Street level residences may be accessed directly from the street, with secondary access to those units from the primary entrance to the upper floors.
 - (c) The primary entrance to the stacked apartments building shall face and be entered from the highest ranking street. See Sec.2.05 for Street Hierarchy Plan.
 - (d) Residences shall be accessed through a ground level lobby/courtyard connected to a system of corridors leading to the units.
 - (e) Each level of building shall have access to a garage (if applicable) via stairs/elevator.
3. Parking Standards
 - (a) All parking for stacked apartment buildings shall be in accordance with Sec.4.03 C, in addition to the following:
 - (b) On-Site parking shall be accommodated via surface parking, tuck under parking or a combination thereof Table 41.
 - (c) Off-site parking may be accommodated via surface parking, on-street parking or a combination thereof Table 41.
4. Services Standards
 - (a) All services for stacked apartment buildings shall be in accordance with Sec.4.03 D, in addition to the following:
 - (b) Services shall be located out of view of the street and shall not impact the general aesthetic of the architecture of the building.
 - (c) For buildings with street frontages on multiple sides, services should be located to the rear of the lot or screened on the lowest ranking street with an architectural wall, solid fence or landscaped hedge. See Sec.2.05 for Street Hierarchy Plan.
5. Private Open Space Standards
 - (a) All stacked apartment buildings shall be meet the private open space standards set forth in Sec.4.03

Figure 47

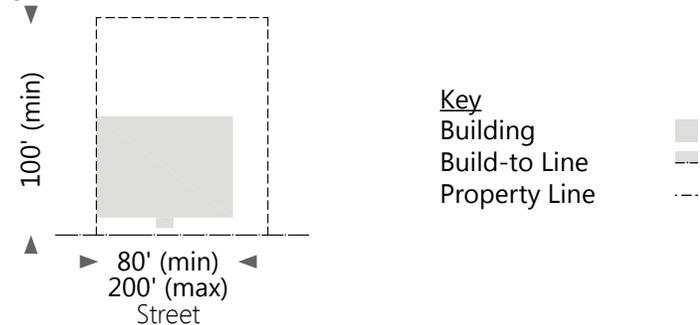


Table 41

On-Site and Off-Site Parking Options

On- Site Parking		Key	Allowed	Non-Allowed
Parking Structure	<input type="checkbox"/>			
Surface Parking	<input checked="" type="checkbox"/>			
Tuck Under Parking	<input checked="" type="checkbox"/>			
Parking Garage (Private)	<input type="checkbox"/>			
Surface Parking Pad	<input type="checkbox"/>			
Off- Site Parking				
Parking Structure	<input type="checkbox"/>			
Surface Parking	<input checked="" type="checkbox"/>			
On-Street Parking	<input checked="" type="checkbox"/>			

D. Stacked Apartment Building continued...

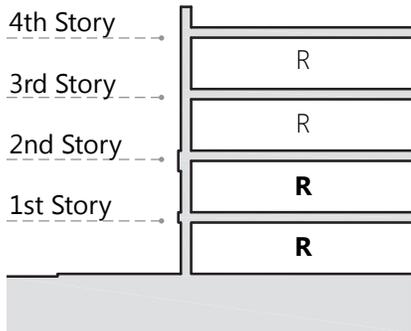
- E, in addition to the following:
 - (b) All multi-family residential shall provide a minimum of 10% of the site for common, private open space.
- 6. Landscape Standards
 - (a) All stacked apartment buildings shall meet the landscape standards set forth in Sec.4.03 F, in addition to the following:
 - (b) Within the areas designated private open space, tree requirements for the stacked apartment building typology shall be minimum 20 trees per acre of lot area.
 - (i) a maximum of 35% of the total tree requirement within the private open space on a lot may be met by a payment-in-lieu under the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Space Program.
- 7. Frontage Standards
 - (a) All frontage for stacked apartment buildings shall be in accordance with Sec.4.03 G, in addition to the following:
 - (b) All stacked apartment buildings shall have a street level frontage type allowed by sector, according to Table 42.
 - (c) In the stacked apartment building, only habitable, building space shall be oriented towards the street, at the ground level.
 - (i) More private rooms, such as service, sleeping or bathrooms shall be oriented away from the street frontage.
- 8. Building Size and Massing
 - (a) All stacked apartment buildings shall be in accordance with Sec.4.03 H, in addition to the following:
 - (b) Buildings may be composed of a primary volume with secondary massing element(s) of reduced volume attached to it.
 - (i) Each building shall meet requirements for each sector as provided in Sec.3.02 A-D
 - (c) The building shall occupy a minimum percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
 - (d) Between the second and third story, buildings shall provide unique architectural detailing/fenestration that unifies all buildings, horizontally, on a lot by lot basis, regardless of building type, at the street elevation.
 - (e) Each building shall be composed of floors programmed with the uses identified in Figure 48.
 - (i) Buildings with multiple street frontages shall provide fenestration on all street fronting sides.
 - (ii) A minimum of 30% of the total building facade must be fenestrated with windows along all street frontages.
 - f. Each unit within the building group, regardless of

Table 42 Permitted Frontage Types by Buildings

Stacked Apartment	DV	DG	UV	NV
Arcade	-	-	○	○
Storefront	-	-	○	○
Forecourt	-	-	○	○
Stoop	-	-	●	●
Porch	-	-	○	●

Key
 Permitted ●
 Non-Permitted ○
 N/A -

Figure 48



Permitted Uses by Floor

<u>Key</u>	
Commercial-Retail	C-R
Office	O
Residential	R
Minimum Stories	
Optional Stories	

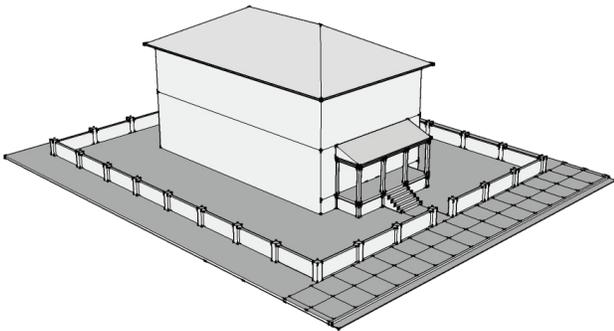
D. Stacked Apartment Building continued...

- use shall have outdoor exposure and access to open space.
- 9. Accessory Structures
 - (a) Accessory structures shall not be permitted.
- 10. Accessory Dwellings
 - (a) Accessory dwellings shall not be permitted.
- 11. Lighting Standards
 - (a) All stacked apartment buildings shall be in accordance with Sec.4.03 K.

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Figure 49

Single Family House Massing



E. Single Family House Building

A residential building type that accommodates one primary residence on all floor(s) of the structure and occupies the totality of the site in and of itself.

1. Lot Width and Depth
 - (a) All single family house building types shall be in accordance with Sec.4.03 A, in addition to the following:
 - (b) The minimum lot width to accommodate a single family house unit along the primary street frontage shall be 45 feet and the maximum lot width shall be 100 feet. The depth of the lot shall be 100 feet minimum.
2. Access Standards
 - (a) All single family houses shall be accessed in accordance with Sec.4.03 B, in addition to the following:
 - (b) The primary entrance to the single family house shall face and be entered from the highest ranking street. See Sec.2.05 for Street Hierarchy Plan.
3. Parking Standards
 - (a) All parking for single family house buildings shall be in accordance with Sec.4.03 C, in addition to the following:
 - (b) On-Site parking shall be accommodated via tuck under parking, private parking garage or surface parking pad Table 43.
 - (c) Off-site parking may be accommodated via on-street parking Table 43.
 - (d) Private garage may be attached or detached to the residence and shall not face onto the primary street frontage.
4. Services Standards
 - (a) All services for single family house buildings shall be in accordance with Sec.4.03 D, in addition to the following:
 - (b) For buildings with frontages on multiple sides, services should be located to the rear of the lot or screened on the lowest ranking street. See Sec.2.05 for Street Hierarchy Plan.
 - (c) Services shall be located at least 10' behind the front of the facade and screened from the street with a solid fence or landscape hedge.
5. Private Open Space Standards
 - (a) All single family house buildings shall be meet the private open space standards set forth in Sec.4.03 E, in addition to the following:
 - (b) All single-family residential shall provide a minimum of 10% of the lot for private open space.
6. Landscape Standards
 - (a) All single family house buildings shall meet the landscape standards set forth in Sec.4.03 F, in addition to the following:
 - (b) Within the areas designated private open space, tree requirements for the single family house building typology shall be minimum 20 trees per

Figure 50

Minimum Lot Size

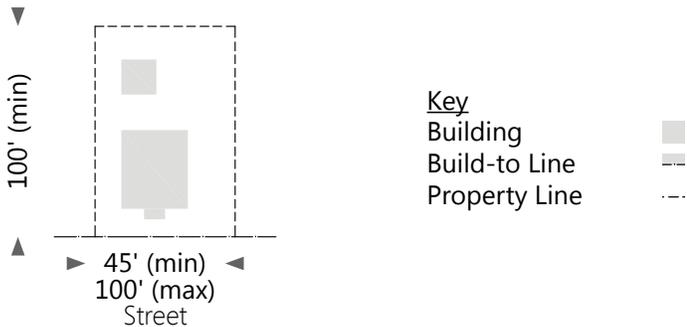


Table 43

On-Site and Off-Site Parking Options

On- Site Parking		Key	Allowed	Non-Allowed
Parking Structure	<input type="checkbox"/>			
Surface Parking	<input type="checkbox"/>			
Tuck Under Parking	<input type="checkbox"/>			
Parking Garage (Private)	<input type="checkbox"/>			
Surface Parking Pad	<input type="checkbox"/>			
Off- Site Parking				
Parking Structure	<input type="checkbox"/>			
Surface Parking	<input type="checkbox"/>			
On-Street Parking	<input type="checkbox"/>			

Table 44 Permitted Frontage Types by Buildings

Single Family House	DV	DG	UV	NV
Arcade	-	-	○	○
Storefront	-	-	○	○
Forecourt	-	-	○	○
Stoop	-	-	●	●
Porch	-	-	○	●

Key
 Permitted ●
 Non-Permitted ○
 N/A -

E. Single Family House Building continued...

acre of lot area.

- (i) a maximum of 35% of the total tree requirement within the private open space on a lot may be met by a payment-in-lieu under the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Space Program.

7. Frontage Standards

- (a) All frontage for single family house buildings shall be in accordance with Sec.4.03 G, in addition to the following:
- (b) All single family house buildings shall have a street level frontage type allowed by sector, according to Table 44.
- (c) In the single family house building, only habitable, building space shall be oriented towards the street, at the ground level.
 - (i) More private rooms, such as service, sleeping or bathrooms shall be oriented away from the street frontage or on the floors above.

8. Building Size and Massing

- (a) All single family house buildings shall be in accordance with Sec.4.03 H, in addition to the following:
- (b) Buildings shall be composed of one (1) and/or two (2) story volume.
 - (i) Each building shall meet requirements for each sector as provided in Sec.3.02 A-D
- (c) The building shall occupy a minimum percentage of primary frontage along the street, at the build-to line, by sector, according to Table 32.
- (d) Each building shall be composed of floors programmed with the uses identified in Figure 51.
 - (i) Buildings with multiple street frontages shall provide fenestration on all street fronting sides.
 - (ii) A minimum of 30% of the total building facade must be fenestrated with windows along all street frontages.

9. Accessory Structures

- (a) Accessory structures shall be permitted in accordance with Sec.4.03 I.

10. Accessory Dwellings

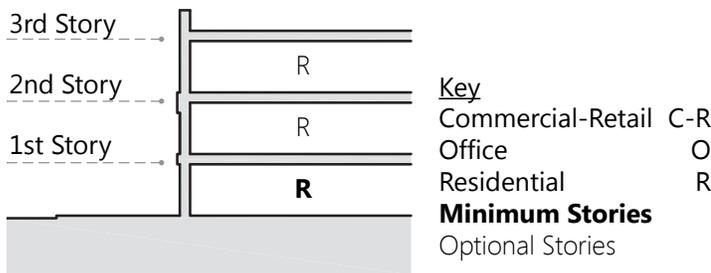
- (a) Accessory dwellings shall be permitted in accordance with Sec.4.03 J.

11. Lighting Standards

- (a) All single family house buildings shall be in accordance with Sec.4.03 K.

Figure 51

Permitted Uses by Floor



Section 4.05 Permitted Frontage Types

Subject to the requirements of the applicable sector, building types shall be designed with one of the permitted frontage types, to ensure that development within the Village of Palmetto Bay meets and addresses the street in accordance with the Village's ultimate vision for the built environment.

A. Arcade

The arcade is a facade application with a street level colonnade at the build-to line, open to the street sidewalk, and with building program on the floors above. Ideal for a commercial/retail use in an urban setting, and complimentary to the storefront frontage type, the arcade frontage type shall only be permitted in certain building types, Sec.4.04 A-B 7(b), and only on the primary frontage along Franjo Road.

B. Storefront

The storefront is a facade placed flush to build-to line, with entrance to the unit at sidewalk level. Ideal for commercial/retail frontage, but also suitable for some higher intensity residential buildings with common entrances. This frontage type can be accompanied by a cantilevered roof(s)/awning(s). Recessed entryways are also accepted in the storefront frontage type. When complimentary to another frontage type, the storefront frontage type may be set back off of the build-to line.

C. Forecourt

The forecourt is a semi-public, exterior open space, compatible with the arcade and storefront frontage type, that is partially surrounded by building on at least two (2) sides and also opened to the street sidewalk, forming a court. The forecourt is appropriate in the form of outdoor landscaped open space/gathering area and suitable for commercial/retail, office or residential uses.

D. Stoop

The stoop is an elevated entry pad, that can encroach into the setback and corresponds directly to the entrance of a building or individual unit. An elevated ground story ensures additional privacy for windows and doors. This frontage type is ideal for residential uses at the ground floor, and also compatible with some lower intensity commercial/retail frontages. When building facade is set back, a decorative fence, shall be required at the build-to line, to preserve the street edge.

E. Porch

Associated with single-family houses, the porch frontage is an elevated semi-private, exterior space, that is built at the setback line and corresponds to the front of a single family house building. The landscape yard space of the setback transitions to an elevated landing before entrance into the building. A fence or shall be built at the build-to line to enclose the yard space and preserve the street edge.

Section 4.06 Specific Parameters by Frontage Type

Figure 52

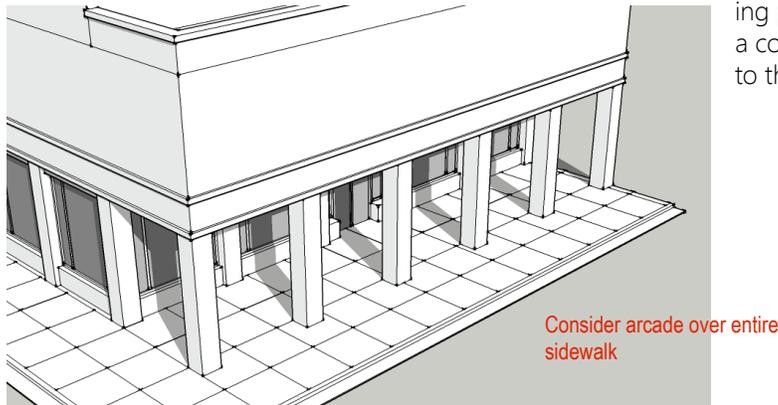
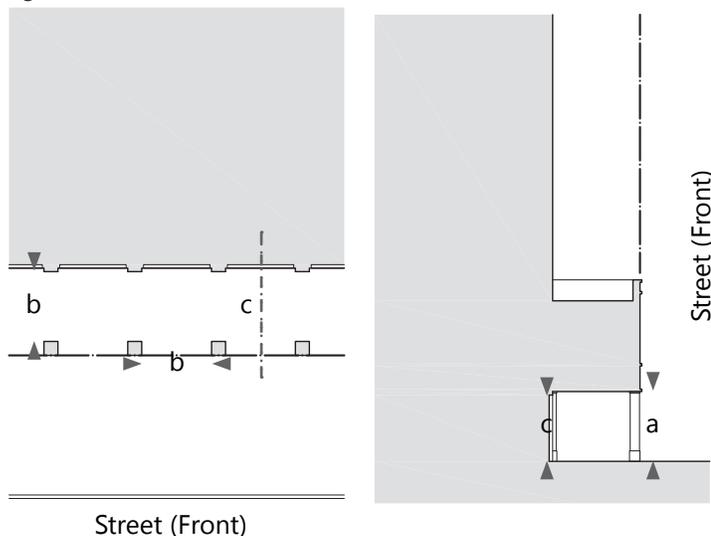


Figure 53



A. Arcade

The arcade is a facade application with a street level colonnade at the build-to line, open to the street sidewalk, and with building program on the floors above. This frontage type is ideal for a commercial/retail use in an urban setting, and complimentary to the storefront and forecourt frontage type.

1. The arcade frontage type shall be permitted with certain building types in the given sector, and only along the primary frontage along Franjo Road, Sec.4.04 A-E 7(b).
2. Soffits, columns, arches/openings and other details shall be treated consistent with the architectural character of the whole building.
3. The openings of the arcade shall be of vertical proportion and have a finished floor that matches at the adjoining sidewalk.
 - (a) The minimum unobstructed clear height, from finished floor to the highest point of arcade opening shall be 14 feet.
 - (b) The minimum clear width from column face to column face, or column face to building face shall be no less than 10 feet.
 - (c) Openings within the storefront shall be aligned to the centerline between the columns and a minimum 10 feet tall.
 - (i) Openings within the storefront shall meet all other requirements for a storefront frontage type. See Sec.4.05 B.
4. Elements may project off the facade of the frontage subject to the standards for encroachments by sector in Sec.3.02 A-D (4)
5. Awnings shall be a maximum 10 feet in width and shall only correspond to and attach to the openings of the arcade.
 - (a) Awnings shall be a minimum one (1) foot clear from the edge of the building in elevation.

Figure 54

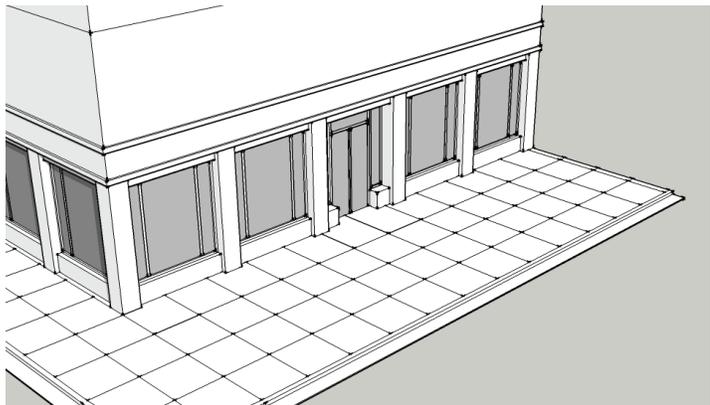
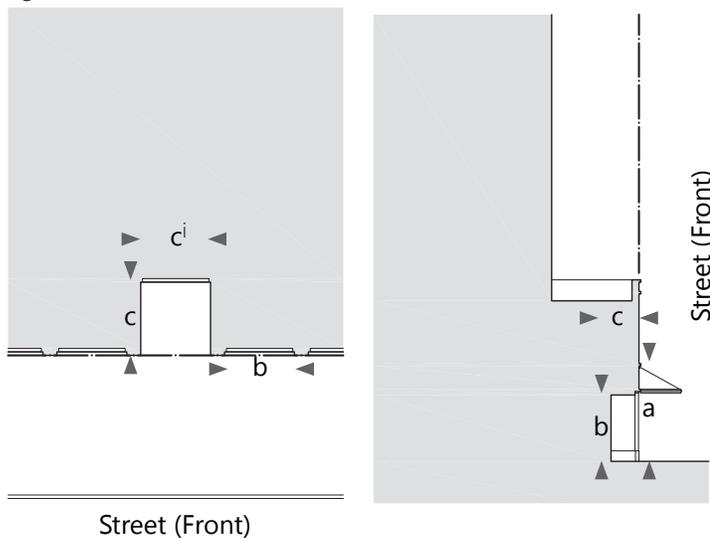


Figure 55



B. Storefront

The storefront is a facade placed flush to build-to line, with entrance to the unit at sidewalk level. Ideal for commercial/retail frontage, but also suitable for some higher intensity residential buildings with common entrances. This frontage type can be accompanied by a cantilevered roof(s)/awning(s). Recessed entryways are also accepted in the storefront frontage type. When complimentary to another frontage type, the storefront frontage type may be set back off of the build-to line.

1. The storefront frontage type shall be permitted with certain building types in a given sector. See Sec.4.04 A-E 7(b).
2. The following shall apply to all storefronts as independent frontage types or complimentary to another frontage type, such as with the arcade or forecourt types:
 - (a) A minimum 14 feet clear to a maximum 18 feet tall, as measured from the finished floor/adjoining sidewalk.
 - (b) Openings within the storefront shall be vertically proportioned and a minimum of 10 feet wide and 10 feet tall.
 - (i) A minimum of 70% of the ground floor storefront shall be glazed with a transparent, non-opaque/non reflective glazing to provide clear view into the unit.
 - (ii) Display cases or merchandise/goods storage shall be 36 inches maximum in height from the finished sidewalk and shall maintain a minimum of 70% clear view into the unit.
 - (iii) Security measures, such as gates, grating or roll down shutters shall be prohibited from exterior application and shall only occur on the interior side of the glass and shall be minimum 50% clear view into the storefront.
 - (c) Entrances to storefront may be recessed eight (8) feet maximum from the build-to line, when used as an independent frontage type.
 - (i) Width of recessed entrance shall be the width of one (1) storefront bay or 10ft maximum, whichever is less.
3. Elements may project off the facade of the frontage subject to the standards for encroachments by sector in Sec.3.02 A-D (4).
4. Awnings shall be a maximum 10 feet in width.
 - (a) Awnings shall only cover storefront openings, not entire facade.
 - (b) For spans wider than 10 feet, a break of eight (8) inches shall be provided between awnings.
 - (c) Awnings shall be a minimum one (1) foot clear from the edge of the building in elevation.

Figure 56

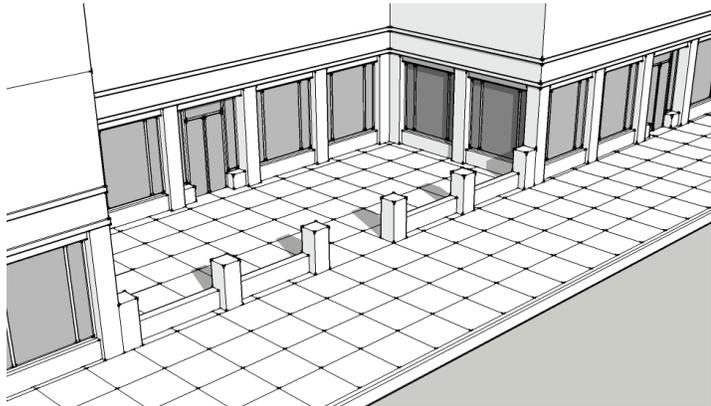
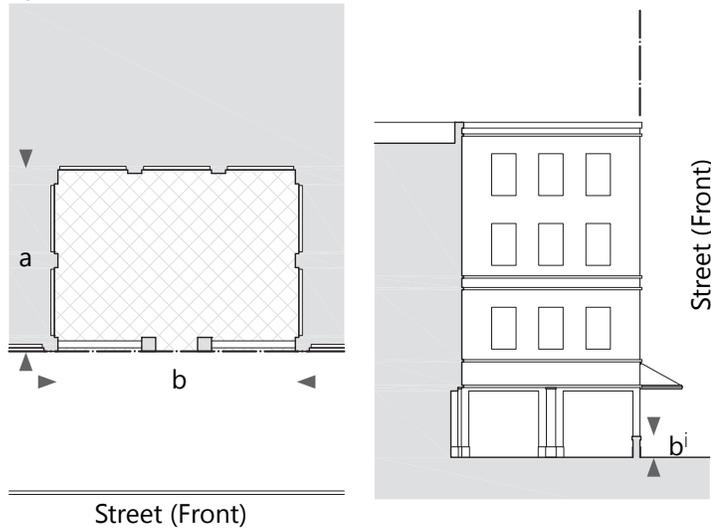


Figure 57



C. Forecourt

The forecourt is a semi-public, exterior open space, compatible with the arcade and storefront frontage type, that is partially surrounded by building on at least two (2) sides and also opened to the street sidewalk, forming a court. The forecourt is appropriate in the form of outdoor landscaped open space/gathering area and suitable for commercial/retail, office or residential uses.

1. The forecourt frontage type shall be permitted with certain building types in a given sector. See Sec.4.04 A-E 7(b).
2. The following shall apply to all buildings with forecourt in conjunction with another frontage type, such as with the arcade or storefront types:
 - (a) A minimum 10 feet to a maximum 40 feet deep along the primary frontage
 - (b) A minimum 20 feet wide and maximum 30% of the lot width along the frontage
 - (i) A decorative fence, maximum three (3) feet in height, may be placed along the build-to line and count towards minimum frontage % standards for a building along the primary street.
3. Openings within the storefront shall meet all other requirements for a storefront frontage type. See Sec.4.05 B.
4. Elements may project off the facade of the frontage subject to the standards for encroachments by sector in Sec.3.02 A-D (4).
5. Awnings shall be a maximum 10 feet in width.
 - (a) Awnings shall only cover storefront openings, not entire facade.
 - (b) For spans wider than 10 feet, a break of eight (8) inches shall be provided between awnings.
 - (c) Awnings shall be a minimum one (1) foot clear from the edge of the building in elevation.

Figure 58

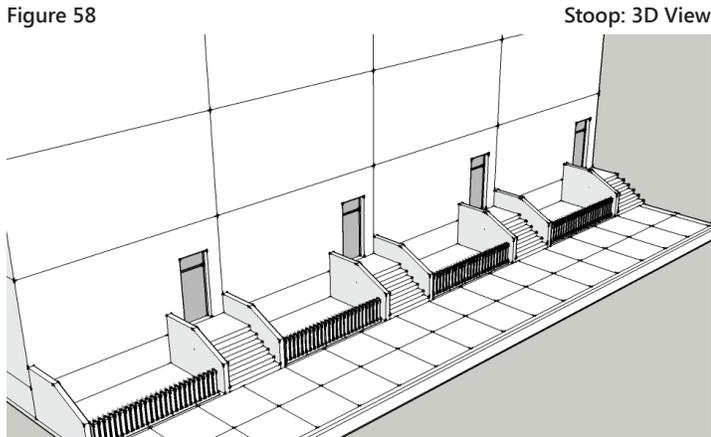
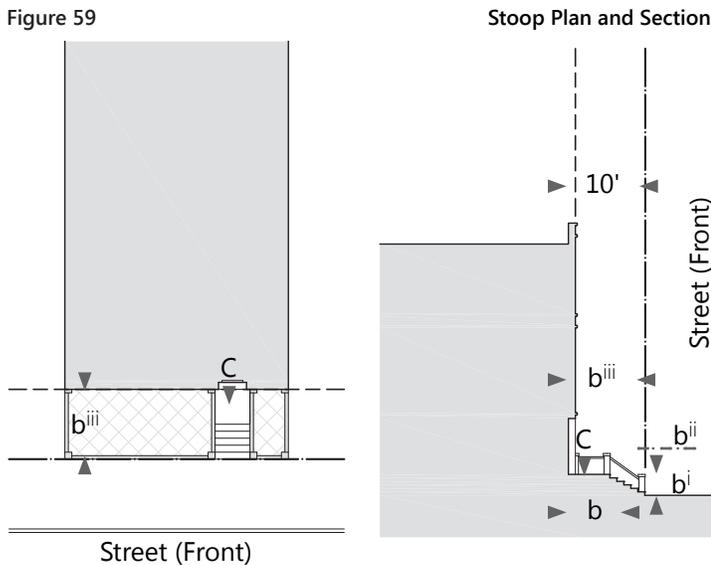


Figure 59



D. Stoop

The stoop is an elevated entry pad, that can encroach into the setback and corresponds directly to the entrance of a building or individual unit. An elevated ground story ensures additional privacy for windows and doors. This frontage type is ideal for residential uses at the ground floor, and also compatible with some lower intensity commercial/retail frontages. When building facade is set back, a decorative fence, shall be required at the build-to line, to preserve the street edge.

1. The stoop frontage type shall be permitted with certain building types in a given sector. See Sec.4.04 A-E 7(b).
2. The following shall apply to all buildings with a stoop frontage type:
3. Stoops may encroach a maximum of eight (8) feet into the build-to line when used on street frontages along Franjo Road.
 - (a) For all other buildings with a stoop frontage, set back shall be 10 feet from the build-to line, on the primary frontage.
 - (b) Stoop may encroach a maximum of eight (8) feet into the setback along all streets excluding Franjo Road.
 - (i) A decorative fence, maximum three (3) feet in height, may be placed along the build-to line and count towards minimum frontage % standards for a building along the primary street.
 - (ii) Area between the interior face of the fence and facade of building shall be landscaped and count towards private open space requirements.
 - (c) Entry pad must be a minimum four (4) feet deep, four (4) feet wide and correspond directly with the entry to the building.
 - (d) Stoop shall transition from adjoining sidewalk level to that of the entry pad, which meets the first floor of the building at the entrance.
 - (i) first floor of building shall be raised a maximum of two (2) feet above the sidewalk level.
4. Elements may project off the facade of the frontage subject to the standards for encroachments by sector in Sec.3.02 A-D (4).
5. Awnings shall be a maximum 10 feet in width.
 - (a) For spans wider than 10 feet, a break of eight (8) inches shall be provided between awnings.
 - (b) Awnings shall be a minimum one (1) foot clear from the edge of the building in elevation.

Figure 60

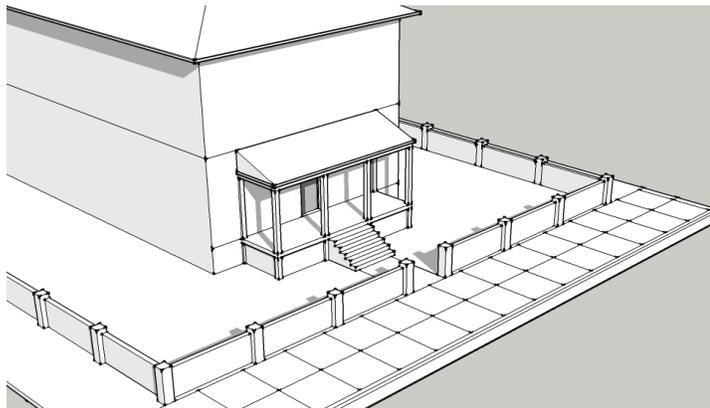
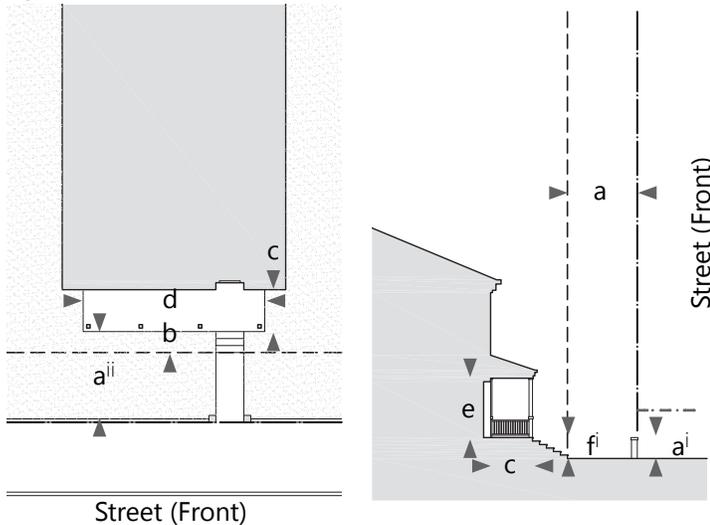


Figure 61



E. Porch

Associated with single-family houses, the porch frontage is an elevated semi-private, exterior space, that is built at the setback line and corresponds to the front of a single family house building. The landscape yard space of the setback transitions to an elevated landing before entrance into the building. A fence or shall be built at the build-to line to enclose the yard space and preserve the street edge.

1. The porch frontage type shall be permitted with certain building types in a given sector. See Sec.4.04 A-E 7(b).
2. Porch frontage type shall be permitted only in the Neighborhood Village (NV) sector
 - (a) For all single family houses with a porch frontage types, setback shall be 10 feet from the build to line, on the primary frontage.
 - (i) A decorative fence, maximum three (3) feet in height, may be placed along the build-to line and count towards minimum frontage % standards for a building along the primary street.
 - (ii) Area between the interior face of the wall and porch shall be landscaped and count towards private open space requirements.
 - (b) Porch shall not encroach into 10 foot setback area to preserve yard space.
 - (c) Porch shall be a minimum six (6) feet deep.
 - (d) A minimum 12 feet wide and correspond directly to the entry of the single family house.
 - (e) A height of minimum eight (8) feet clearance from finished floor of porch to ceiling.
 - (f) Porch shall transition from adjoining sidewalk and yard level to meet the first floor of the building at the entrance.
3. Elements may project off the facade of the frontage subject to the standards for encroachments by sector in Sec.3.02 A-D (4).
4. Awnings shall be a maximum 10 feet in width.
 - (a) For spans wider than 10 feet, a break of eight (8) inches shall be provided between awnings.
 - (b) Awnings shall be a minimum one (1) foot clear from the edge of the building in elevation.

30-50.23.5 Street Connectivity Standards

Section 5.01 Purpose

This section identifies the standards, by which all streets both new and existing, shall be met with regards to the dedication, construction and/or redevelopment by both the Village of Palmetto Bay and its individual property owners, in addition to any other public entities/stakeholders.

All construction of new and redevelopment of existing right-of-ways shall be the responsibility of the individual property owners and are intended to support the Village of Palmetto Bay’s future vision for a highly connected, multi-modal, ped/bike-friendly, network of streets within the Downtown Urban Village (DUV). Property owners shall be responsible for the portion of the right-of-way on all sides of development, considered street frontage.

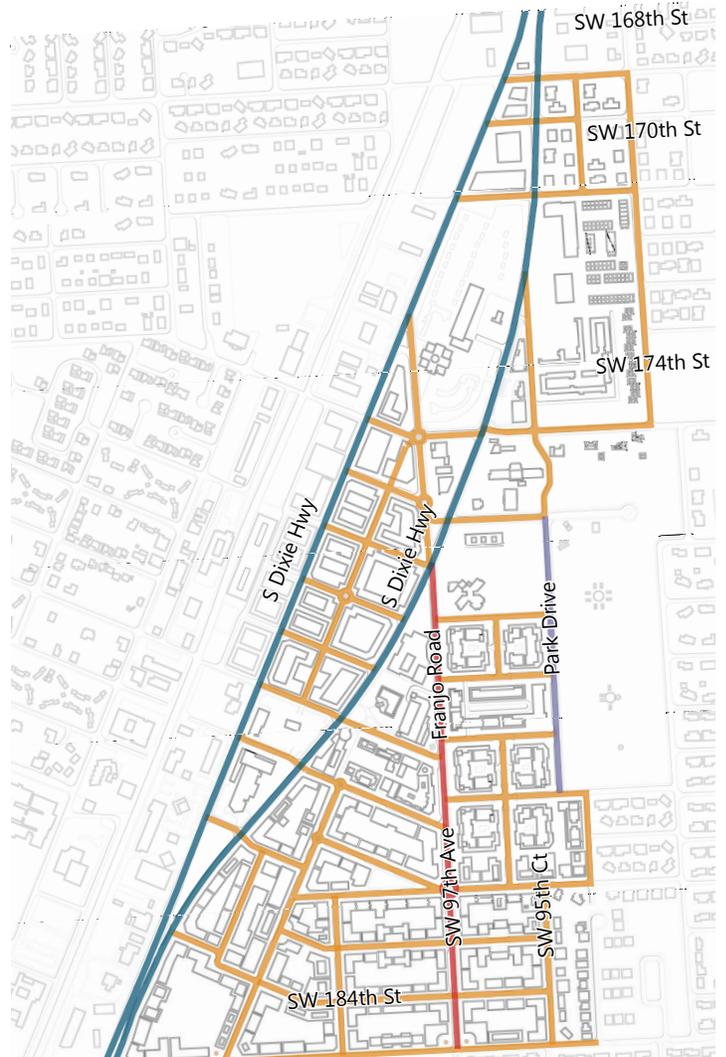
The intention of this section is to provide the tools necessary for property owners and potential developers to determine the type of street and the elements within the right of way necessary to achieve the Village of Palmetto Bay’s vision for the Downtown Urban Village (DUV). The size, location and treatment of the elements that compose the right of way shall determine the relevant build-to line, in which property owners and developers shall base plans for their parcels and apply all parameters of development.

Key

(FR)	Franjo Road	
(US1)	US-1	
(P)	Park Drive	
(TS)	Typical Street	

Figure 62

Street Type Plan



A. Franjo Road (FR)

Figure 63

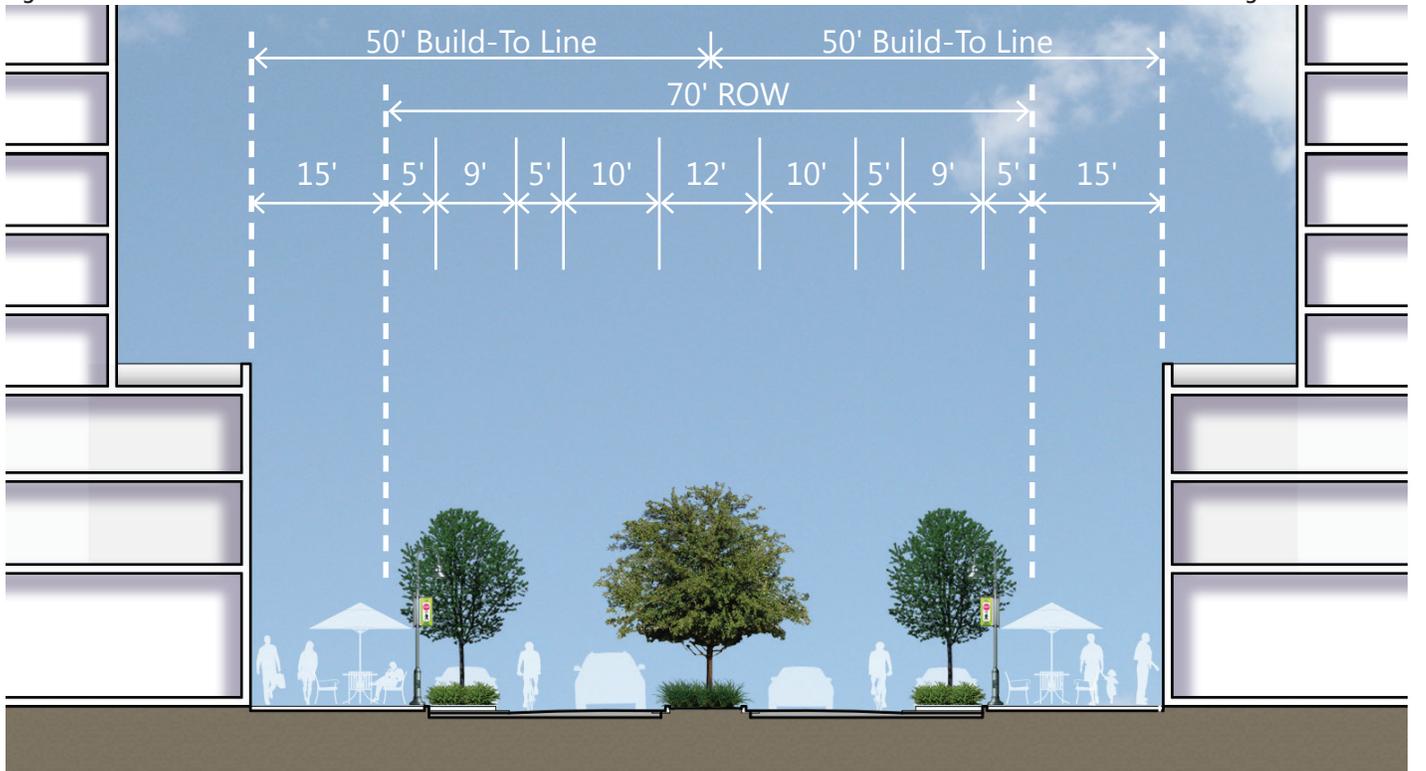


Table 45

Elements of the ROW

		Sectors			
		DV	DG	UV	NV
Street Type	Franjo Road	■	■	■	□

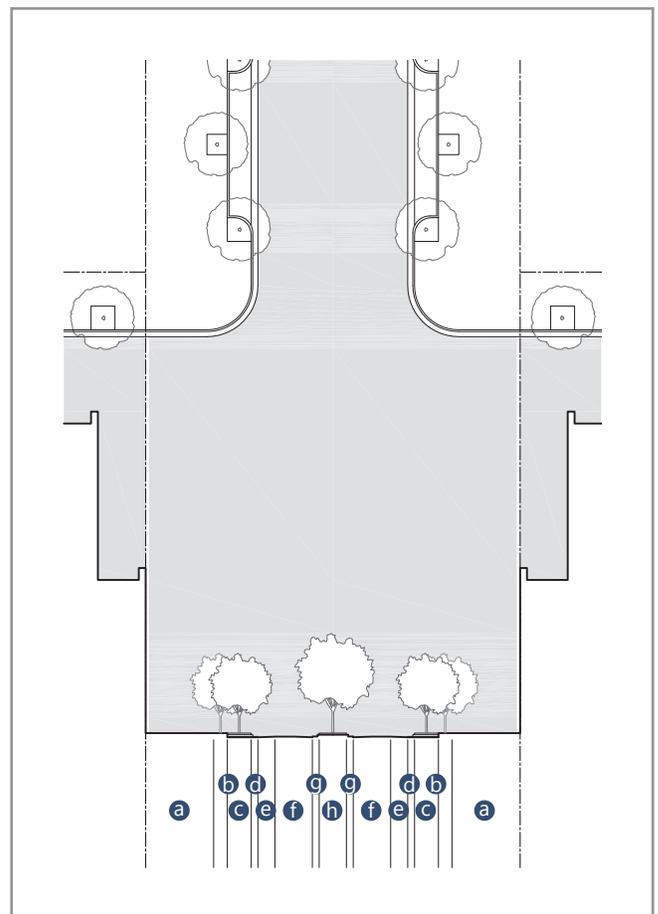
ROW Elements	Type
(a) Pedestrian Way	Sidewalk
(b) Landscape Area	Trees (Integrated)
(c) Parking Area	Parallel Parking
(d) Pavement Transition	Curb and Gutter
(e) Bicycle Lane	One-Way (Striped)
(f) Travel Lane	One-Way (Vehicular)
(g) Pavement Transition	Curb and Gutter
(h) Median Area	Landscaped (with Left Turn)

Key
 Allowed ■
 Non-Allowed □
 N/A -

- At a minimum, there shall be provided a minimum ROW of 70', 35' from the centerline of the road at all times.
- All ground floor uses along Franjo Road shall be of commercial/retail uses in accordance with Sec.2.07.

Figure 64

Elements of the ROW



B. US-1 (US1)

Figure 65

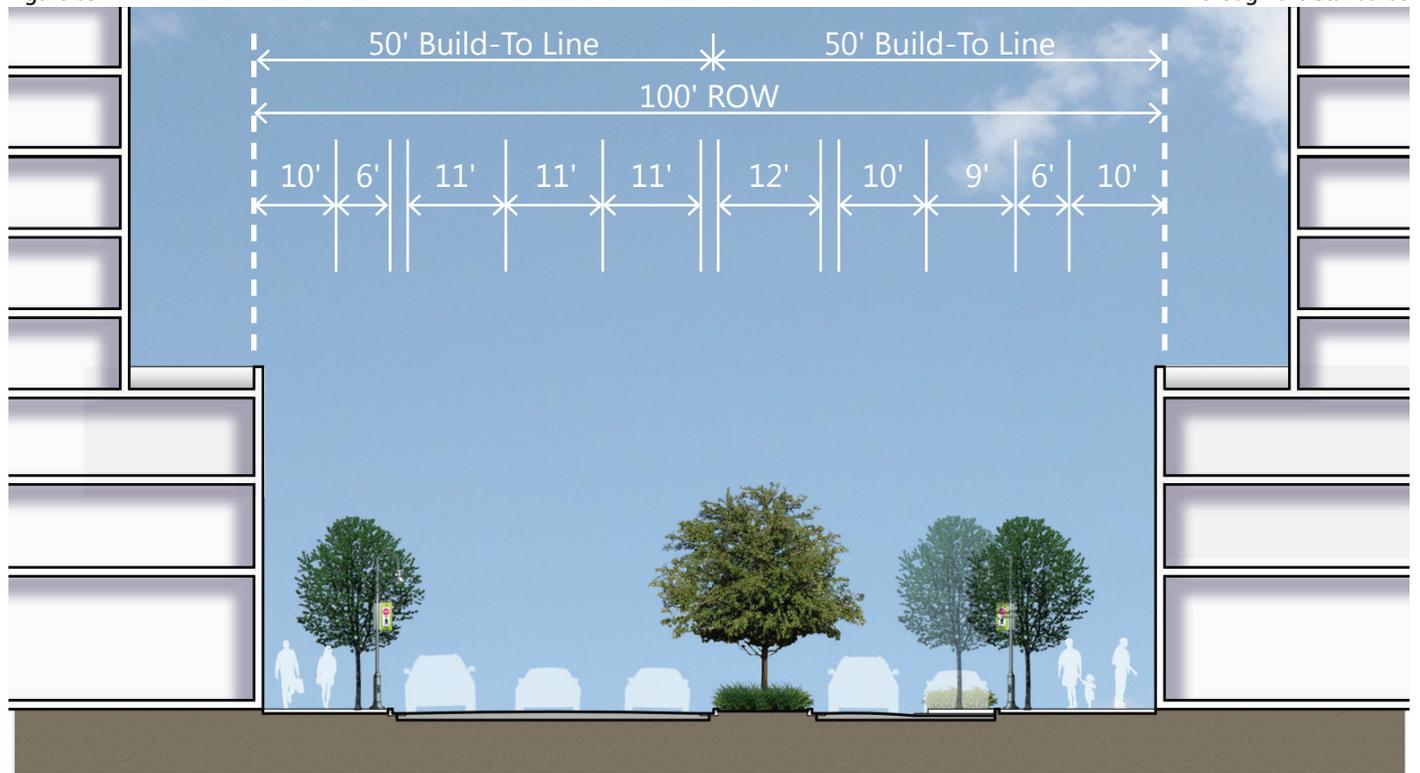


Table 46

Elements of the ROW

Street Type		Sectors			
		DV	DG	UV	NV
US1	US-1	■	■	□	□

ROW Elements	Type
(a) Pedestrian Way	Sidewalk
(b) Landscape Area	Tree Grate
(c) Parking Area	Parallel Parking (1 Side)
(d) Pavement Transition	Curb and Gutter
(e) Bicycle Lane	-
(f) Travel Lane	One-Way (Vehicular)
(g) Pavement Transition	Curb and Gutter
(h) Median Area	Landscaped

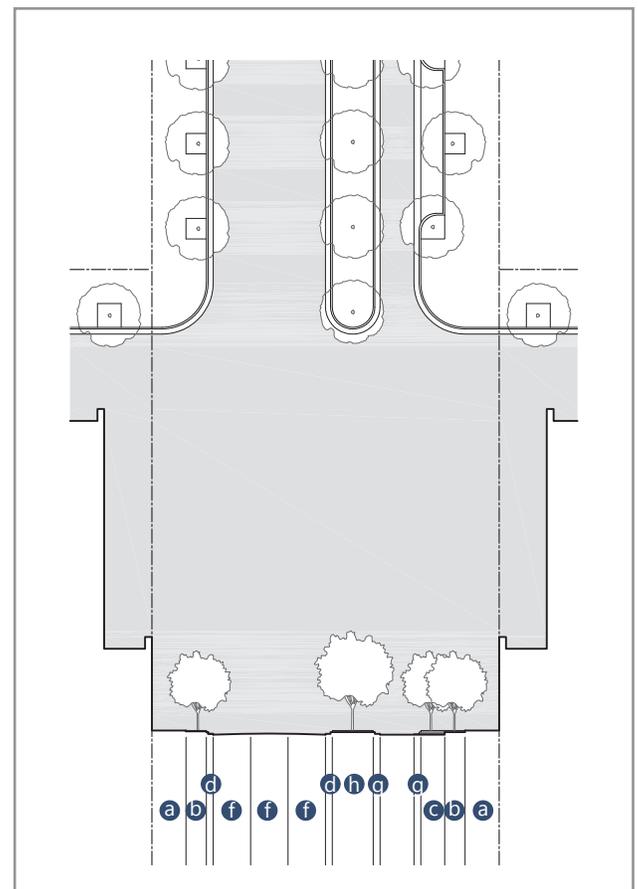
Key

Allowed
 Non-Allowed
 N/A

1. For southbound lanes of US-1, where ROW is less than 100', setback and build-to line shall be 16'.
2. Slip lanes shall not be required for Southbound lanes of US-1.

Figure 66

Elements of the ROW



C. Park Drive (P)

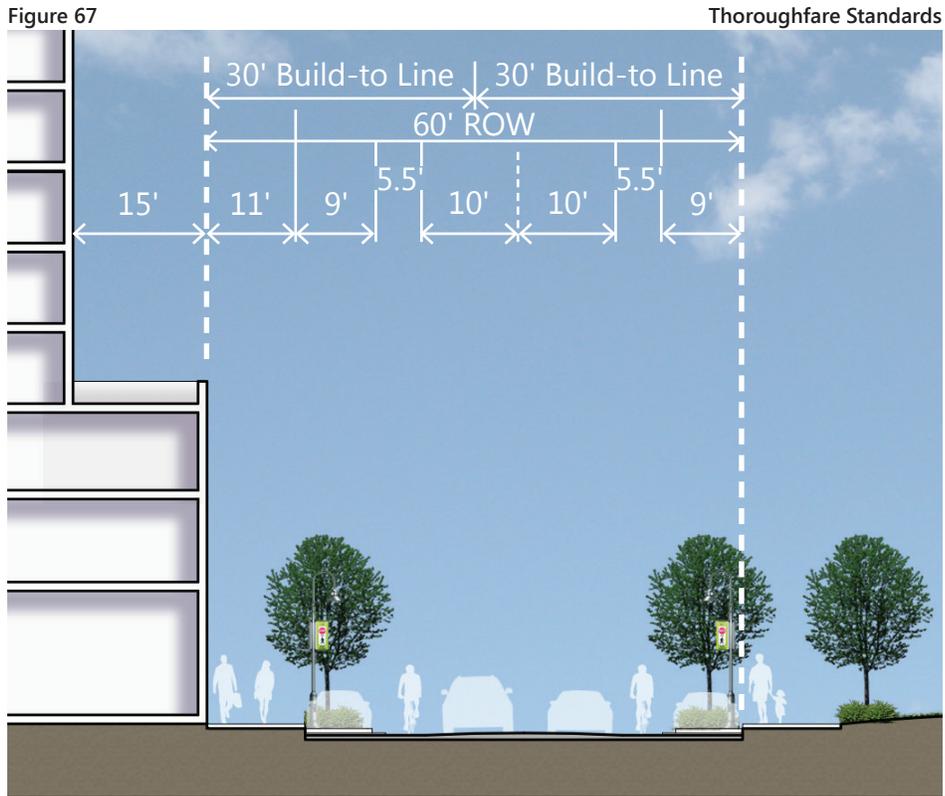


Table 47

Elements of the ROW

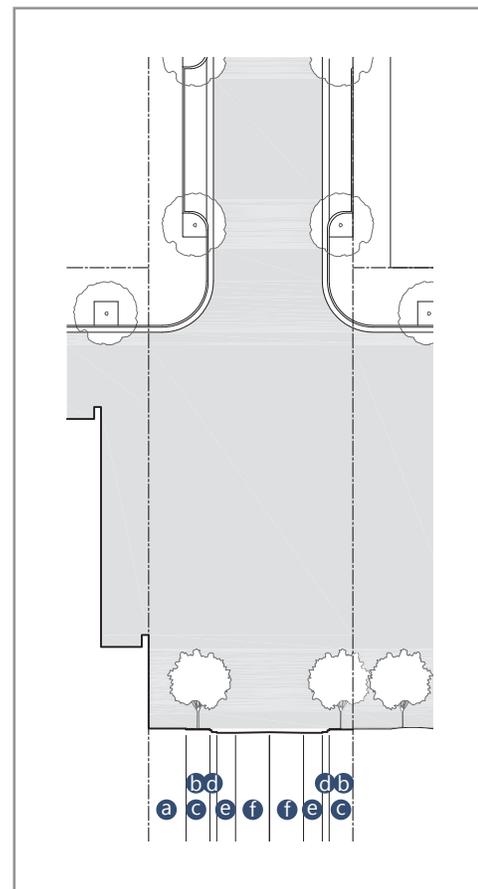
Street Type		Sectors			
		DV	DG	UV	NV
P	Park Drive	■	□	□	□

ROW Elements	Type
(a) Pedestrian Way	Sidewalk
(b) Landscape Area	Trees (Integrated)
(c) Parking Area	Parallel Parking
(d) Pavement Transition	Curb and Gutter
(e) Bicycle Lane	One-Way (Striped)
(f) Travel Lane	One-Way (Vehicular)
(g) Pavement Transition	-
(h) Median Area	-

Key
 Allowed ■
 Non-Allowed □
 N/A -

Figure 68

Elements of the ROW



D. Typical Street: Urban 1 (TS-U1)

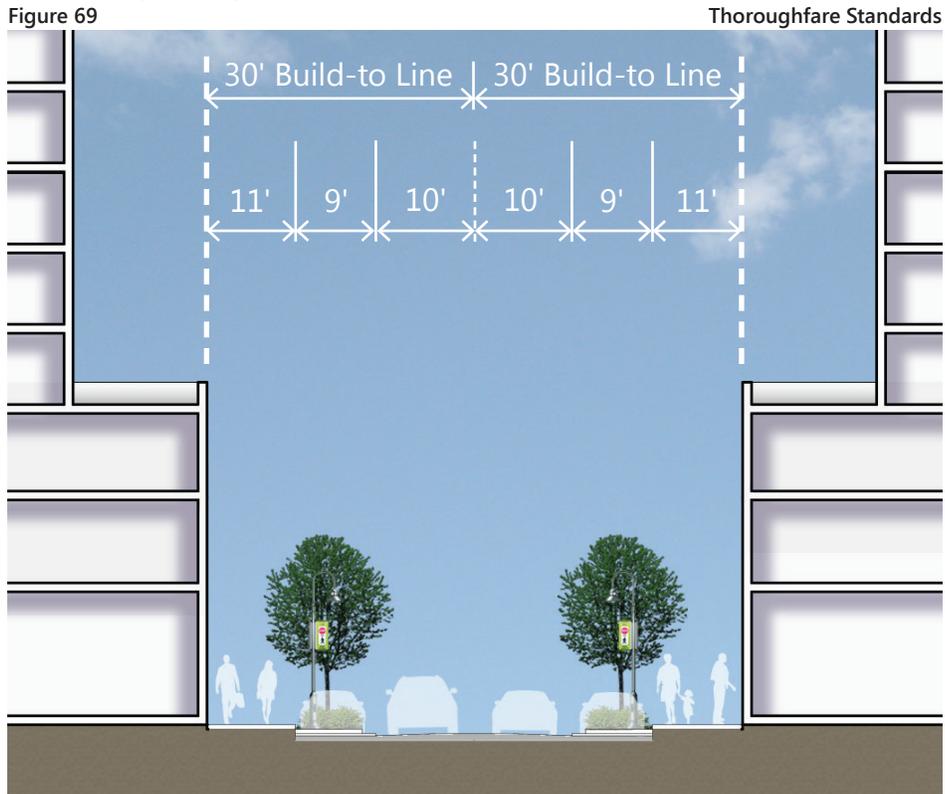


Table 48

Elements of the ROW

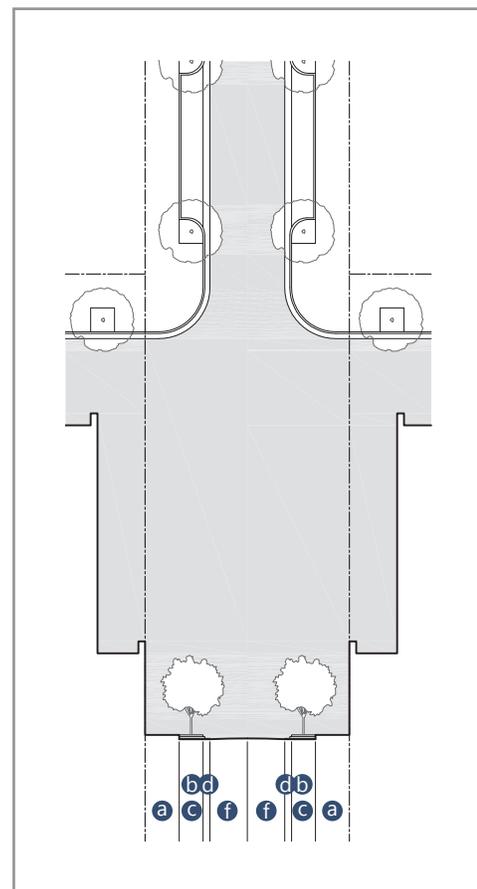
Street Type		Sectors			
		DV	DG	UV	NV
TS-U1	Urban 1	■	■	■	□

ROW Elements	Type
(a) Pedestrian Way	Sidewalk
(b) Landscape Area	Tree Grate
(c) Parking Area	Parallel Parking
(d) Pavement Transition	Curb and Gutter
(e) Bicycle Lane	-
(f) Travel Lane	One-Way (Vehicular)
(g) Pavement Transition	-
(h) Median Area	-

Key
 Allowed ■
 Non-Allowed □
 N/A -

Figure 70

Elements of the ROW



E. Typical Street: Neighborhood 1 (TS-N1)

Figure 71

Thoroughfare Standards

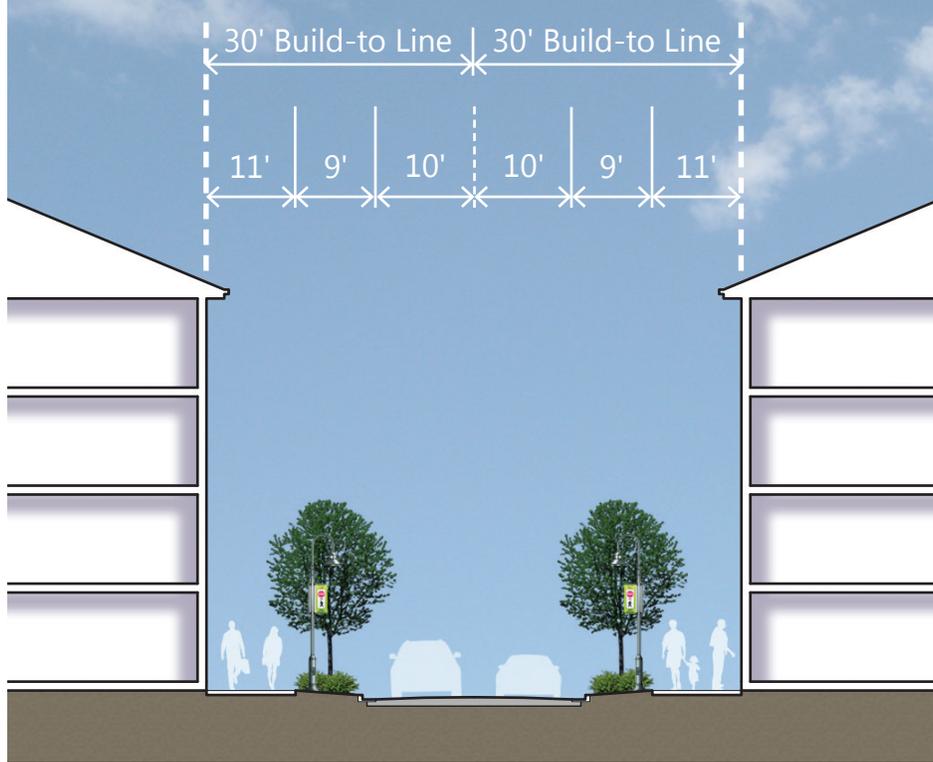


Table 49

Elements of the ROW

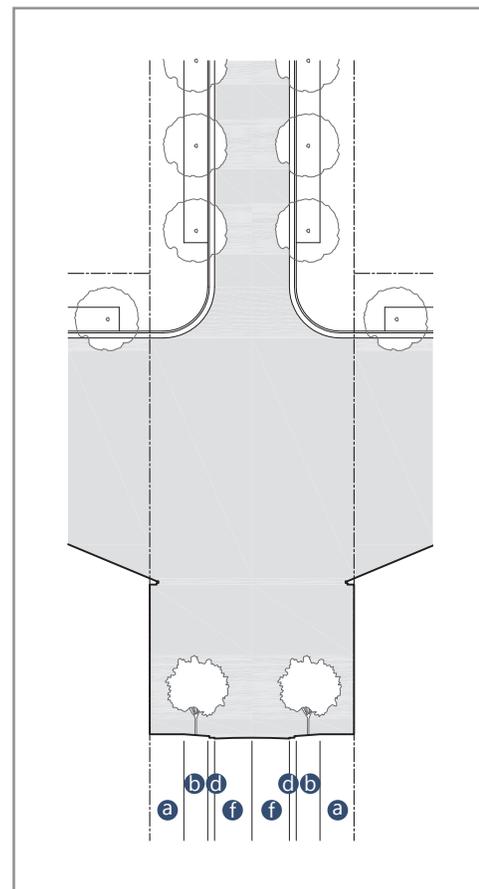
Street Type	Sectors			
	DV	DG	UV	NV
TS-N1 Neighborhood 1	□	□	■	■

ROW Elements	Type
(a) Pedestrian Way	Sidewalk
(b) Landscape Area	Planting Strip
(c) Parking Area	-
(d) Pavement Transition	Curb and Gutter
(e) Bicycle Lane	-
(f) Travel Lane	One-Way (Vehicular)
(g) Pavement Transition	-
(h) Median Area	-

Key
 Allowed ■
 Non-Allowed □
 N/A -

Figure 72

Elements of the ROW



30-50.23.6 DEFINITIONS

Terms used throughout this document shall take their commonly accepted meaning unless otherwise defined in Village of Palmetto Bay Code Sec. 30-40.1 as adopted by the Village of Palmetto Bay. Terms requiring interpretation specific to this chapter are as follows:

Abutting: A common border or being separated from such common border by a roadway, easement or right-of-way.

Access: The place or way by which pedestrians and vehicles have a safe and usable ingress and egress to a site. An unobstructed way or means of approach to provide entry to, or exit from, a property.

Accessory Building: An enclosed building that is subordinate to and not the main or principal building on a lot or parcel and that is used as a dwelling unit, garage, storage shed or similar use.

Accommodation Uses: Facilities that provide short term lodging including: hotels; motels; rooming houses, bed and breakfasts; and similar uses. Accommodations shall be rented in no less than 24 hour (minimum) increments.

Alley: An alley is any public or private thoroughfare for the use of pedestrians or vehicles, 20 feet nor more than thirty 30 feet in width, and is intended for service and only a secondary means of access to abutting properties

Apartment: A multi-family unit type that is for rent.

Arcade: See 'Frontage Types'

Automotive Uses: Establishments specializing in the service or repair of automobiles; automobile tire sales and replacement; automobile parts sales and installation; sales of new and used automobiles; and gas stations or other form of stations used for the powering/charging of automobiles.

Base Element: A continuous raised platform supporting a building, or a large block of two stories beneath a multi-layer block of a smaller area.

Bicycle Lane: An on-road facility specifically dedicated for bicycle use.

Big Box Retail/Services: A chain, commercial-retail establishment with gross floor area greater than 20,000 sq. ft.

Block: A combination of contiguous building lots, the perimeter of which abuts public street(s), private street(s), easement(s) or dedicated open space(s).

Building Frontage: The portion of the building required to be located along the build-to-line.

Building Height: The vertical distance measured from the average height of the crown of the road, adjacent to the building frontage, to the top of the highest slab for a flat roof. For a pitched roof, the height of the building shall be measured to the highest point of the finished roof.

Building Type: A structure defined by the combination of mass, configuration and placement, within a site.

Build-to-Line: A line established by the Street Connectivity Standards and determined by the street it abuts, which is parallel to the block face, along which the building shall be built.

Civic Uses: Uses that are accessible to the public and serves the religious, recreational, educational, cultural and/or governmental needs of the community. Civic Uses include, but are not limited to: convention/meeting halls; private clubs; libraries; police stations; fire stations; post offices; clubhouses; religious buildings; museums; athletic facilities; auditoriums; theaters; other performing arts buildings; and government facilities. The architecture of a civic use building shall reflect its civic nature.

Clear View: For commercial-retail uses, unobstructed site line into the units shall be maintained to encourage/generate pedestrian activity/interaction and provide surveillance of the street.

Colleges and Universities: Facilities that serve the educational needs of the adult population. This group shall include universities; colleges; commuter colleges; trade schools and other similar uses as determined by the Director.

Commercial Parking Structure: Structures that provide parking as the primary on-site use. These facilities offer short-term parking of vehicles and may charge a fee for such use. This group includes; shared parking facilities; shuttle parking facilities; transit park-and-ride facilities and other similar uses as determined by the Director.

Community Garden: Open space that is set aside for the cultivation and harvesting of produce such as flowers, fruits and vegetables.

Condominium: An ownership version of a multi-family unit types.

Courtyard House: An attached single-family dwelling type that contains a court or atrium. The court shall be enclosed on at least three sides by habitable building space and shall provide penetrable openings such as windows and doors between the interior of the dwelling and the court. A courtyard house may occupy the maximum frontage as allowed by building type within a sector.

Decorative Fence: A functional fence that is designed with aesthetics in mind and adds to the appearance and design of the property/building. 50% of the square footage of the fence shall be open.

Department: The Village of Palmetto Bay Planning and Zoning Department

Designated Public Open Space: An outdoor, at grade space including greens, squares and plazas, as indicated on the Public Open Spaces Plan. Designated open spaces may also be set aside by property owners who wish to participate in the Palmetto Bay Downtown Urban Village (DUV) Landscape and Open Spaces Program.

Director: The director of the Village of Palmetto Bay Planning and Zoning Department.

Drive-Through Facilities: Drive-through facilities associated with retail use, personal service establishment or restaurants.

Dwelling Unit Type: One of three (3) multi-family residential unit types: Flat; Loft; or Townhouse; each of which can be classified as an Apartment or Condominium.

Entertainment Uses: Uses in this group shall include; night-clubs; coin arcades; movie theaters; performance theaters; radio, movie and/or television studios; billiard halls; skating rinks; bingo halls; piano bars; bowling alleys and similar uses as determined by the Director. The sale of alcohol shall be ancillary to the primary entertainment use.

Entrance (Main): The principal point of access of pedestrians to a building. In the support of ped/bike activity, the main or primary entrance shall be oriented to the frontage rather than to the parking.

Flat: a single (1) story dwelling unit, occupied by one (1) single-family.

Fenestration: Design and position of windows and other structural openings within a facade.

Flexible Block Building: See 'Building Type'

Flex Building: See 'Building Type'

Floor Plate: The shape and size of any given floor of a building. The floorplate that touches the ground is called the footprint, after the shape that it leaves on the land.

Food and Beverage Establishments: Uses in this group shall include; full service restaurants; fast food restaurants; bars and pubs; and similar uses as determined by the Director.

Forecourt: See 'Frontage Types'

Frontage Type: The architectural element that serves to transition from the public right-of-way to the entrance of a building type. Frontage type, when combine with the public realm and building type create the desired streetscape.

Front Property Line: The property line that runs parallel to the highest ranking street as identified in the Street Hierarchy Plan.

General Retail/Personal Services: Establishments that provide goods and services geared toward an individual consumer. This group shall include businesses such as: banks; beauty parlors; adult day care; bakeries; bookstores; apparel stores; grocery stores; pharmacies; health clubs; gift shops; indoor pet care/boarding and indoor kennels (soundproof and air conditioned required); vehicle retail showrooms; and similar uses as determined by the Director. This group shall also include schools offering instruction in dance, music, martial arts and similar activities as determined by the Director.

Green: An outdoor open space that shall not be hard surfaced for more than 20% of the area exclusive of dedicated streets. The landscape shall consist of primarily lawn, trees and garden structures.

Group Residential Home: A dwelling unit, licensed by the State of Florida Department of Children and Families that serves resident clients and provides a living environment for unrelated residents who operate as a functional equivalent of a family. Uses in this group shall include: nursing homes; assisted living facilities; congregate living facilities; foster care facilities; community residential homes; group homes; or other similar uses as determined by the Director. Services that support the daily operation of group homes are permitted and shall include dining facilities, doctor's offices, nurse's offices, staff offices, recreation rooms and similar facilities and services.

Habitable Building Space: Air-conditioned space, the use of which involves regular human presence. Habitable space shall not include areas devoted to parking, storage, service room, private spaces or corridors.

Horizontal Projection: The distance in which an architectural element can project off of the facade of a building.

Landscape Area: Area within the right-of-way that identifies the type of landscape finishing applied to the ground, between the pedestrian way and pavement transition.

Liner Building: Building configuration, shallow in depth, no less than 20' and occupied with habitable space to screen a

parking garage or surface parking lot from the public realm.

Loft: a double-story height dwelling unit with or without mezzanine, occupied by one (1) single-family.

Lot Width: The length of a parcel along the primary frontage.

Lot Depth: The length of a parcel along the interior side or on the secondary frontage.

Maximum Base Density: The maximum residential dwelling units/acre permitted on a site without applying density unit increases from the Village of Palmetto Bay's Reserve Units Pool and/or through TDR.

Maximum Height with Bonus: The maximum permitted height in stories, of a building with any applicable bonus(s) stories.

Median Area: Landscaped area within the right-of-way that can accommodate landscaping in between travel lanes on a boulevard.

Mezzanine: An intermediate floor, between stories of a building, that does not count against the number of stories, so long as it is no bigger than 30% of the area of the main story below.

Municipal Recreation Facility: A building, playground or park, owned/operated by the Village, County, State or the Federal Government.

Mixed-Used Building: A building that includes a combination of residential and non-residential uses or two different non-residential uses, vertically integrated, such as: retail/office uses at the ground floor and residential on the floors above.

Neighborhood Proprietor Commercial-Retail and Office Services: Small scale, non-chain businesses, operated on the first floor of a building type, by the owner, such as: hobby shops, tailor or beauty shops, photography studios, bakery cafe shops or other similar uses/offices, as determined by the Director. The use shall specifically preclude the use of large machinery or the creation of noxious odors/ambient noise levels that exceed the levels for that area, as provided in the Village's noise ordinance and the sale of alcohol shall be ancillary to the primary uses permitted.

Office Uses: Facilities used primarily for the business of professionals with only limited transactions occurring on-site. This group shall include offices for : accountants; architect; appraisers; attorneys; consulates; financial firms; insurance adjusters; realtors; medical offices and other uses as determined by the Director.

Off-site: The outside limits of the area encompassed by the lot where a permitted activity is conducted.

Off-site parking: Any parking structure, surface parking or on-street parking located on a development parcel or public right of way other than the parcel being developed.

On-Site Parking: Any parking structure, surface parking, tuck under parking, private parking garage or surface parking pad within the property lines and applicable build-to lines on private property.

On-Street Parking: Parking on a private or public right-of-way street. On-street parking spaces shall be head-in, diagonal or parallel parking, according to the street type parameters for the right-of-way.

Parking Area: Area within the right-of-way that includes on-street parking. Also, the parking area will serve as to transition and protect the pedestrian way from the travel lanes.

Parking Structure: A multi-level, publicly accessible building with the primary use to accommodate the parking requirements for both residential and non-residential uses.

Pavement Transition: Transition between the Pedestrian way and travel lanes, within the right-of-way. Can be curb and gutter or swale, as identified in thoroughfare standards.

Pedestrian Paseo: Pedestrian-only passage meant to break up the mass of large buildings a mid-block locations, allowing access to the lot behind buildings and connecting directly from the network of sidewalks and openspaces.

Pedestrian Way: Area within the right of way that is designated as the primary area for pedestrian movement.

Porch: See 'Frontage Types'

Plaza: An outdoor open space fronted by mixed-use retail and office uses. A minimum of 50% and a maximum of 75% of the plaza's area, exclusive of dedicated streets, shall be hard surfaced. The landscape of plazas shall consist primarily of hard-surfaced areas, permanent architecture or water features and trees that are placed in an orderly fashion.

Premium Transit Station: A public transportation station that is served by modes of public transportation such as heavy rail, light rail or at a minimum, express bus rapid transit routes.

Primary Frontage: For property with multiple frontages, the edge of the property that fronts the highest ranking street as identified in the Street Hierarchy Plan.

Private Open Space: Any form of courtyards, balconies, terraces, lawns, community gardens, amenity recreation decks and landscaped roof terraces/gardens on buildings/parking structures. In addition, the area of any covered patio, gazebo or other roofed shade structures shall count towards meeting the private open space requirements, as long as two (2) sides are opened to the outside.

Private Parking Garage: A private parking structure that can accommodate parking requirements for a single family residential uses such as single-family houses, or rowhouse building types, located at the rear of the lot, away from the primary street frontage.

Reserve Commercial Square Footage: The developable commercial square footage which is available for allocation by the Village, which is in excess of the base square footage identified by the Comprehensive Plan.

Reserve Residential Units: The residential units identified within the Comprehensive Plan, which are available for allocation by the Village, beyond that permitted by the Base Maximum Density within a given sector of the DUV.

Rowhouse Building: See 'Building Type'

Sender site: The designated lot that sends/transfers residential development unit rights, to a lot identified as the receiver site.

Single Family House Building: See 'Building Type'

Square: An outdoor open space that shall be flanked by streets on at least three (3) sides and shall not be hard-surfaced for more than 50% or the area exclusive of dedicated streets. Squares shall be landscaped primarily of hard-surfaced walks, lawns and trees that are placed in an orderly fashion.

Stacked Apartment Building: See 'Building Type'

Stoop: See 'Frontage Types'

Storefront: See 'Frontage Types'

Story: The habitable space between finished floor and finished ceiling.

Street: Any thoroughfare, such as a public street, private street, or easement that affords primary access to an abutting property.

Street Frontage: The edge of the property that abuts a street. **See Primary Frontage**

Street Network: A system of intersecting and interconnect-

ing streets and service roads.

Surface Parking: A one-layer parking lot at the ground level that accommodates parking requirements for both residential and non-residential uses.

Surface Parking Pad: a private, surface parking lot that accommodates the parking requirements for single-family residential uses and located at the rear of the lot, or away from primary street frontage.

Street Vista: A view through or along a street centerline terminating with the view of a significant visual composition of an architectural structure or element.

Townhouse: a two (2) or more story dwelling unit, occupied by one (1) single-family.

Transfer of development rights (TDR): The procedure by which development rights to construct residential units may be transferred from one lot within the Downtown Urban Village (DUV) zoning district to another lot within the Downtown Urban Village (DUV) zoning district.

Travel Lane: Area within the right-of-way dedicated for motor vehicles that can also be shared with bicyclists, as identified in thoroughfare standards.

Tuck Under Parking: Parking spaces integrated on the surface level of a site, where habitable building program cantilevers on the floors above.

Thoroughfare Standards: Design criteria that establish the required elements for the placement and size of the following: sidewalks; curbs and gutters; parking; medians; bike lanes; traffic lanes; street trees and landscaping.

Vertical Clearance (Ground): An area measured from the finished sidewalk, which shall be kept clear of all objects to the prescribed height for pedestrians to pass under.

Vertical Proportion: a proportion that is at a minimum the same width that it is tall. Preferably the height of the subject is greater than the width.

Appendix:

How to use this document?

The following steps indicate, in general, the intended use of this document, as follows:

1. Identify development site on all regulating plans. See Sec.2.02-2.06

With a survey of the development site:

2. Identify Street Types in Sec.5.01
 - (a) Verify and Identify any new streets in Sec.2.03 applicable to the site
 - (b) Set aside applicable street sections identified in step 2. See Sec.2.05 A-E.
 - (c) Identify Primary Frontage in Sec.2.05
3. Establish build-to lines according to the street type parameters that are identified and set aside in Step 2.
 - (a) Yields buildable area within development site
4. Identify applicable sector for development site. See Sec.2.02.
 - (a) set aside urban design standards applicable to sector identified for development site. See Sec.3.02 A-D.
5. Verify permitted uses within applicable sector. See Sec.2.07
6. Verify the permitted building types within applicable sector. See Table 30.
 - (a) set aside general development parameters applicable to all buildings. See Sec.4.03.
7. Identify building type intended to be developed within site Sec.4.02
 - (a) set aside the specific development parameters by building type, identified in step 7. See Sec.4.04 A-E.
8. Verify permitted frontage types for individual buildings within applicable sector. See Sec.4.04 A-E 7(b).
9. Identify frontage type intended to be applied to individual building type identified. See Sec.4.05 A-E
 - (a) set aside specific parameters by frontage type
10. Design and develop site plans and architecture subject to the standards and parameters set aside.

SECTION II: WHAT WE HEARD

The team of consultants conducted a series of one-on-one interviews, resident meetings, a public workshop and electronic communication (e-mail) to seek input regarding the DUV. Throughout this process, generally referred to as a “listening tour,” the consultant team made no specific observations or recommendations, nor did it support or refute any statement expressed. All suggestions and expressions of hope and concern for the future of the Village are valid statements, and have been carefully considered in the creation of this report.

In light of the amendments proposed and to facilitate the successful revision of the DUV, the consultants believe it is important to establish a common base and agreed-upon principles. Towards that end certain statements expressed during the process that directly relate to planning policies and principles key to the successful implementation of the community’s vision are addressed and factually clarified or refuted in this section.

A summary of comments is listed below.

“STOP ALL INCREASES”

VALID CONCERN. The DUV does not clearly articulate or quantify the bonus program. Increases, incentives and bonuses should be halted until clearly defined and quantified in the code.

“WE NEED TO FIND A COMPROMISE”

TRUE. Amending the DUV does not mean you can ignore as-of-right entitlements. Amending the DUV does not entail starting over with a “clean slate”. There are property rights that cannot be taken back. However, it is possible to yield base rights and densities with varying form and intensities. Incentives in the DUV are not expressed as base rights.

“HIGH COST OF LAND DRIVES THE NEED FOR MORE HEIGHT”

NOT TOTALLY ACCURATE. Land values are usually tied to supply and demand and specific market conditions. In some cases, high cost of land is the result of the desirability of a location or the established high yield of a site. However in communities actively pursuing redevelopment, high cost of land is often linked to speculation, lack of predictability or over-entitlement in early phases of implementation. It is important to differentiate between the relationship of height and density with land values. Investors will seek an increased number of units to balance high land costs. But the taller the building, the higher the construction cost and the more units required to balance the equation. Add high parking requirements into the mix and development becomes so expensive that height is an avoidable consequence. Furthermore, it is possible to yield high densities in low-rise buildings. (Note: the definition of low-rise varies. In this case it refers to 3 to 5 story buildings.)

“WE NEED THE TO HAVE 5389 RESIDENTIAL UNITS TO SUPPORT TRANSIT”

ARBITRARY. While density is critical to the success of transit, it is not clear if the DUV will ever implement 5389 (4,143 + 1246) units. As the regulations exist today, it is also not clear if these units will be in close proximity to a station and therefore supportive of transit.

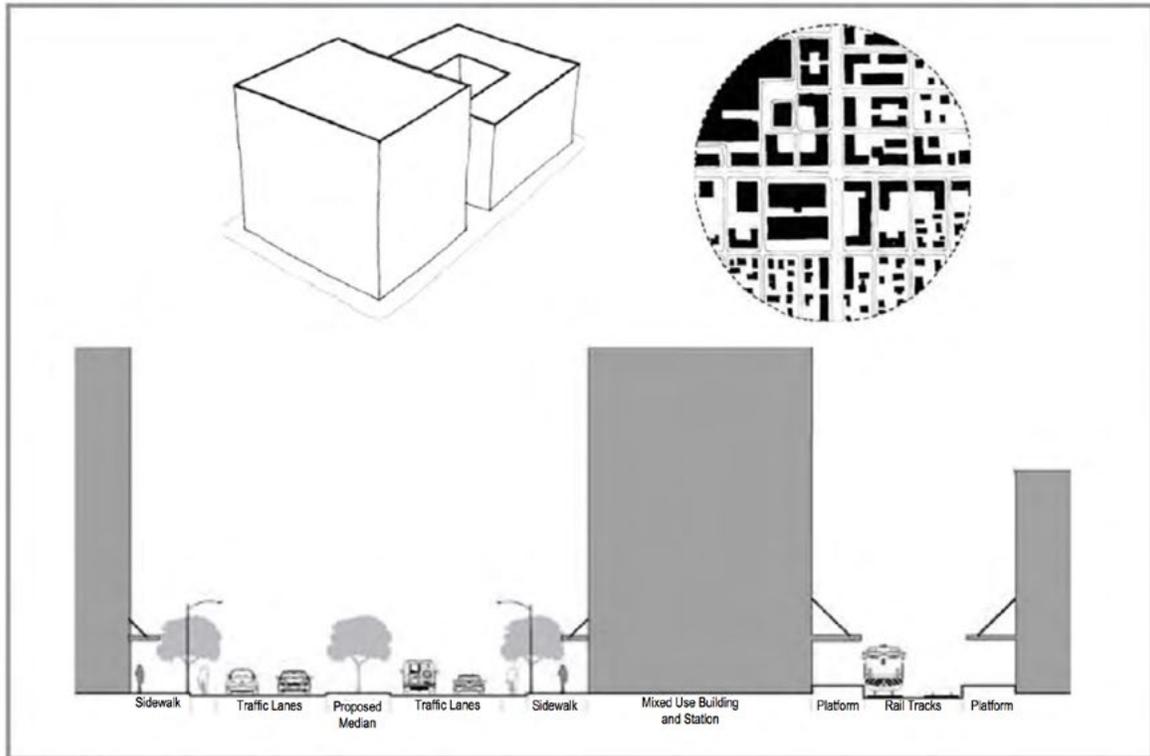
There is no universally accepted set of standards for what constitutes transit-supportive densities. Given the variation in land use and market conditions across regions and station locations, there is no single threshold that is appropriate for all areas. Locally, the Florida Department of Transportation, working with the Treasure Coast Regional Planning Council developed the “Florida Department of Transportation TOD Guidebook” which analyzes transit and stations nationwide and defines types and densities as appropriate parameters for Florida communities.

The TOD Guidebook defines Community Centers as areas that function as sub-regional or local centers of economic and community activity and include town centers served by one or more transit types.

A Framework for TOD in Florida - Regional Center

Table 1-1

		1	2	3	
		Regional Center			
		Heavy Rail	Commuter/Light Rail	Bus Rapid Transit/Bus	
STATION AREA MEASURES	Gross Intensity/Density				
	Station Area Employment and Residential Units	70,000 - 95,000	45,000 - 70,000	23,000 - 45,000	
	Station Area Total Residential Units	10,000 - 15,000	5,000 - 10,000	3,000 - 5,000	
	Gross Residential Density (Dw/Acre)	55 - 75	35 - 55	20 - 35	
	Station Area Total Employment	60,000 - 80,000	40,000 - 60,000	20,000 - 40,000	
	Gross Employment Density (Jobs/Acre)	200 - 250	100 - 200	50 - 125	
	Jobs/Housing Ratio (Jobs/Residential Units)	6 : 1			
SITE LEVEL MEASURES	Mix of Uses				
	Mix of Uses - % Residential / % Non-Residential	35% / 65%			
	Net Intensity/Density				
	Net Commercial Floor Area Ratio (FAR)	4.0 - 6.0	2.0 - 4.0	1.5 - 3.0	
	Net Residential Density (Dwelling Units per Acre)	85 - 115	55 - 85	30 - 55	
	Street Network and Building Design				
	Grid Density - Blocks per Square Mile for Vehicular, Bicycle, and Pedestrian Street Network	> 350	> 350	>230	
	Building Height (in Floors)	> 4	> 3	> 2	
	Maximum Lot Coverage	80% - 90%	80% - 90%	60% - 70%	
	Minimum Street Frontage	80% - 90%	80% - 90%	70% - 80%	
Parking					
Maximum Residential Parking - Spaces per Residential Unit	1	1	1.5		
Maximum Non-Residential Parking - Spaces per 1,000 square feet	1	1	2		
Park & Ride	No	No	No		

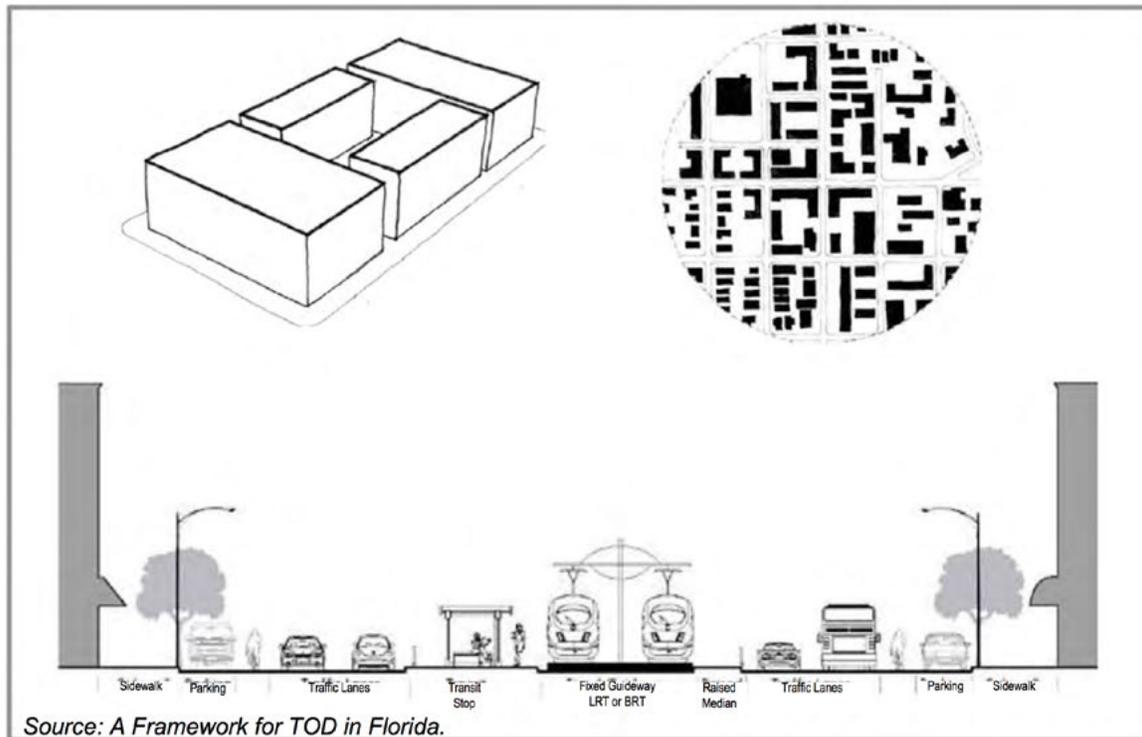


Regional Centers are centers of economic and cultural significance, including downtowns and central business districts, which serve a regional travel market and are served by a rich mix of transit types ranging from high speed, heavy or commuter rail to BRT to local bus service. Usually emphasizing employment uses, Regional Centers increasingly are being sought out for residential uses in response to changing demographics and housing preferences. Regional Centers contain more than one transit station and multiple bus stops. Small block sizes, higher intensities and densities of development, civic open spaces, and minimal surface parking result in a highly urban development pattern in Regional Centers.

A Framework for TOD in Florida - Community Center

Table 1-2

		4	5	6
		Community Center		
		Heavy Rail	Commuter/Light Rail	Bus Rapid Transit/Bus
STATION AREA MEASURES	Gross Intensity/Density			
	Station Area Employment and Residential Units	23,000 - 30,000	15,000 - 23,000	7,000 - 15,000
	Station Area Total Residential Units	5,000 - 6,000	3,000 - 5,000	1,000 - 3,000
	Gross Residential Density (Dus/Acre)	35 - 65	25 - 35	10 - 20
	Station Area Total Employment	18,000 - 24,000	12,000 - 18,000	6,000 - 12,000
	Gross Employment Density (Jobs/Acre)	65 - 90	45 - 65	20 - 45
	Jobs/Housing Ratio (Jobs/Residential Units)		3 : 1	
	Mix of Uses			
	Mix of Uses - % Residential / % Non-Residential		45% / 55%	
	SITE LEVEL MEASURES	Net Intensity/Density		
Net Commercial Floor Area Ratio (FAR)		4.0 - 6.0	2.0 - 4.0	1.0 - 2.0
Net Residential Density (Dwelling Units per Acre)		60 - 80	40 - 60	20 - 40
Street Network and Building Design				
Grid Density - Blocks per Square Mile for Vehicular, Bicycle, and Pedestrian Street Network		> 350	>230	>150
Building Height (In Floors)		> 3	> 2	> 2
Maximum Lot Coverage		80% - 90%	60% - 70%	40% - 50%
Minimum Street Frontage		80% - 90%	70% - 80%	60% - 70%
Parking				
Maximum Residential Parking - Spaces per Residential Unit		1	1.5	2
Maximum Non-Residential Parking - Spaces per 1,000 square feet		1	2	3
Park & Ride		No	No	No



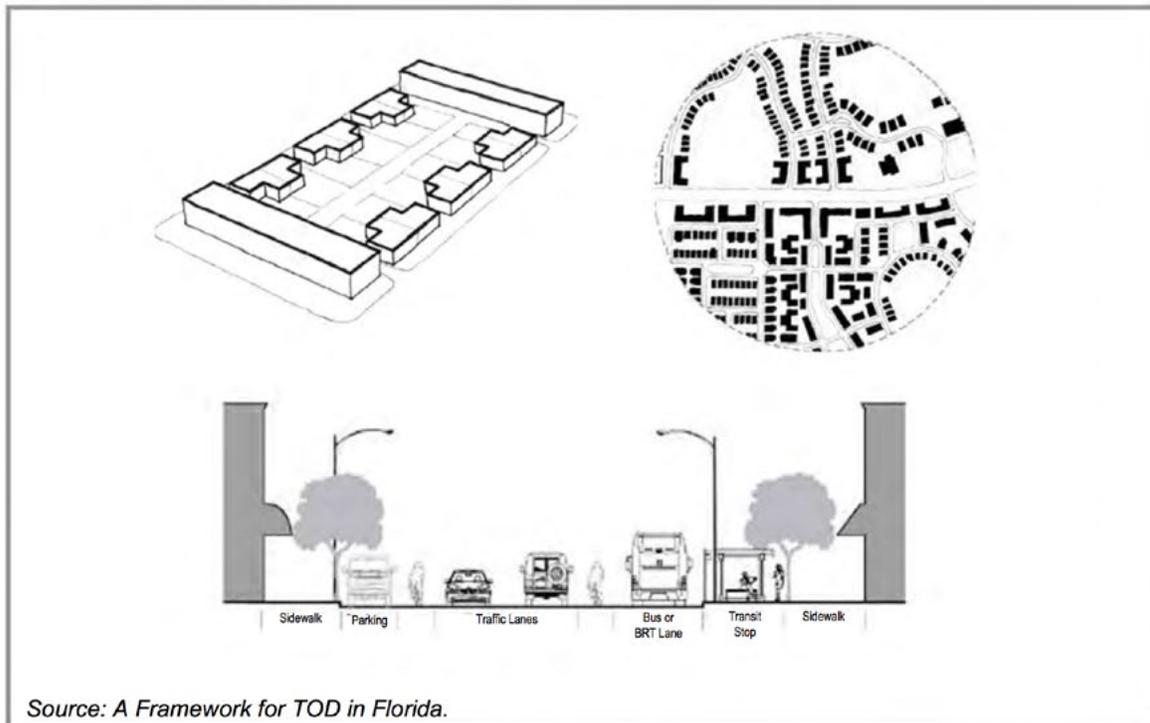
Source: A Framework for TOD in Florida.

According to the TOD Guidelines, the mix of uses in Community Centers is balanced between residential and employment uses. More intense and dense development in Community Centers tends to be concentrated within walking distance of a transit station. The pattern of development in Community Centers ranges from urban to suburban. Block sizes, lot coverage, and development intensities and densities all tend to be moderate. Parking is typically structured and located close to the transit stations. Table 1-2 identifies development targets for Community Center TOD place type. The figure below it illustrates a prototypical Community Center urban form that reflects application of the station area.

A Framework for TOD in Florida - Neighborhood Center

Table 1-3

		7	8	9
		Neighborhood Center		
		Heavy Rail	Commuter/Light Rail	Bus Rapid Transit/Bus
STATION AREA MEASURES	Gross Intensity/Density			
	Station Area Employment and Residential Units	5,000 - 8,000	4,000 - 6,000	2,000 - 4,000
	Station Area Total Residential Units	3,000 - 4,500	2,000 - 3,000	1,000 - 2,000
	Gross Residential Density (Dus/Acre)	12 - 15	9 - 12	7 - 9
	Station Area Total Employment	2,000 - 3,500	2,000 - 3,000	1,000 - 2,000
	Gross Employment Density (Jobs/Acre)	20 - 30	15 - 20	10 - 15
	Jobs/Housing Ratio (Jobs:Residential Units)	1 : 1		
SITE LEVEL MEASURES	Mix of Uses			
	Mix of Uses - % Residential / % Non-Residential	75% / 25%		
	Net Intensity/Density			
	Net Commercial Floor Area Ratio (FAR)	1.5 - 2.0	1.0 - 1.5	0.5 - 1.0
	Net Residential Density (Dwelling Units per Acre)	15 - 20	12 - 15	10 - 12
	Street Network and Building Design			
	Grid Density - Blocks per Square Mile for Vehicular, Bicycle, and Pedestrian Street Network	> 230	> 150	> 150
	Building Height (in Floors)	> 2	> 2	> 1
	Maximum Lot Coverage	60% - 70%	40% - 50%	40% - 50%
	Minimum Street Frontage	70% - 80%	60% - 70%	60% - 70%
Parking				
Maximum Residential Parking - Spaces per Residential Unit	1.5	2	2	
Maximum Non-Residential Parking - Spaces per 1,000 square feet	2	3	3	
Park & Ride	Yes	Yes	Yes	



The TOD Guidebook defines Neighborhood Centers as areas dominated by residential uses served by some type of premium transit. Non-residential uses in them are limited to local-serving retail and services. Residential densities in Neighborhood Centers tend to be lower than in Community Centers and at their highest within walking distance of the transit station. Neighborhood Centers are found in older urban areas, infill redevelopment areas and newer suburban developments. Open space/undeveloped is usually abundant in them, and parking is mostly in surface lots. Table 1-2 identifies development targets for Neighborhood Center TOD place type. The figure below it illustrates a prototypical Neighborhood Center urban form that reflects application of the station area. (Table 1-1)

“DENSITY IS GOOD FOR SHOPS AND BUSINESSES”

TRUE. A mix of housing and offices supports retail as it creates more customers, supports longer business hours and typically is associated with an increase in rents and price per square foot, both for residential and commercial units, (by up to 20% higher than in single-use areas (source ULI)). Office users and their visitors provide daytime retail and restaurant demand, while residents bring customers in the evening and during weekends.

Currently the DUV's requirement and definition of mixed-use and where retail is mandatory is vague. A clear vision/plan for the commercial area (main street) identifying convenient parking, wide and carefully designed sidewalks with appropriate street furniture and lighting and shade is important to entice and support retail. Financial and regulatory tools to attract desired stores/services is also important. It is critical to create a positive investment climate, improve infrastructure and reward investors who further community goals. It is also important to understand the rules and regulations in place and create predictability by applying them fairly, consistently and expeditiously. A community has to be willing to reward projects that further community's goals and protect it from negative influences project or projects that significantly depart from its vision.

“NOBODY WILL WALK 1000' TO TAKE TRANSIT”

FALSE. Given the right conditions, many will. In the past decade, there has been growing market demand for pedestrian and transit-oriented development in cities, reflecting changing demographics and preferences. Walking happens when a community invests in pedestrian infrastructure and pedestrian-oriented design features and uses. Improvement of the pedestrian realm and reduced distances between places to shop, work, and play increases chances of private investment, a stronger economic environment and the creation of more desirable addresses and place. Data shows that given the right conditions and the choice to walk, most people will walk for some of their daily trips. Multiple national studies, including one by the National Association of Realtors® have determined that millennials (who by 2020 will constitute nearly half of the workforce of the entire US) prefer walking over driving by a substantially wider margin than any other generation. Those who can't drive and those who prefer not to will also walk anywhere from 1/4 to 1/2 mile to reach a destination or transit. Bicycles increase this distance.

HIGHER-DENSITY HOUSING IS ONLY FOR LOWER-INCOME HOUSEHOLDS

FALSE. Multifamily housing is not the “housing of last resort for households unable to afford a single-family homes.” People of all income groups and ages seek higher density living. Higher-density development is a viable housing choice as people transition through different phases of their lives. It is also the preferred choice of those looking for convenience and amenity-rich environments.

“BUILDINGS WITH LESS PARKING ATTRACT THE ‘WRONG KIND OF PEOPLE’”

FALSE. Real estate trends show that appropriately located housing near transit without assigned parking is desirable for residents of all incomes, for rental or ownership. Cars and parking are expensive and drive building mass. Eliminating on-site parking brings down the cost of construction and makes it possible for developers to deliver more attainable housing. However zero-parking housing attracts high-income buyers as well. Luxury apartments and condos are being built throughout the country without any car parking. Zero-parking buildings should be carefully considered in areas that are very close to transit and services, weighing contributions to a centralized parking system as well as tight regulations to ensure that there is negligible impact on the community's curbside parking stock.

“MIXED-USE AND HIGH DENSITY WILL GENERATE MORE TRAFFIC AND PARKING PROBLEMS”

FALSE. Well-designed mixed use developments result in walkable environments. Residents of mixed-use areas have the choice to make fewer and shorter trips than those living in lower density areas. Condominium, apartment and townhouse residents in mixed use environments average 5.6 trips per day, compared with 10 trips per day averaged by residents of low density neighborhoods (a trip is defined as any time a car leaves or returns a destination - e.g. a car trip that involves leaving one's home, stopping to drop kids off at school, then at one's work place, back to pick up kids from school and finally stopping at the grocery on the way back home represents 5 trips). Additionally, mixed-use neighborhoods

generate what is known as “park-once environments”: people park their car in one place and accomplish several tasks, which not only reduces the number of car trips required but also reduces overall parking needs for the community. Higher-density, mixed use development also makes public transit feasible, increasing a community’s transportation choices. Such choices are impossible for low-density developments.

“8 STORIES ARE NECESSARY TO GET APPROPRIATE DENSITY TO SUPPORT TRANSIT/ACCOMMODATE RESERVE UNITS”

FALSE. While additional stories should allow for higher densities, it is possible to achieve transit-supportive densities with low to moderate height. The following built examples depict densities significantly higher than those stipulated in the DUV in fewer than 8 stories. Several factors, such as parking, open space and unit size requirements come into play when determining how units “fit” in a building.



Cannery Row - Delray Beach, Florida
18.3 du/ac
Residential



The Mark - Delray Beach, Florida
29 du/ac
Retail/Residential



Civitas
20.5 du/ac - 3 stories
Retail/office/residential



The Meridian
29 du/ac - 3 stories
Residential



CityWalk
29 du/ac - 3 stories
Retail/Residential



Pineapple Grove Village
39.2 du/ac - 4 stories
Residential



Sofa 1
62 du/ac - 4 stories
Residential



Worthing Place
69 du/ac - 6 stories
Limited Retail/Residential

SECTION III: OBSERVATIONS & RECOMMENDATIONS

The Village should be commended for committing to multiple visioning and planning efforts to develop a clear and predictable regulatory environment. These efforts were recognized by the Southeast Florida Smart Growth Partnership with a Project Award of Excellence.

The DUV Form-Based Code was intended as a tool to attract development and investment to the downtown area while preserving and enhancing the character of the single family fabric. Given the increase in development proposals and permit requests, the DUV appears to be meeting a critical objective. However, concerns expressed by some in the community regarding the intensity of new development being proposed, a departure from the envisioned character seen in recent submittals and inconsistencies throughout the document revealed during the review process indicate that a comprehensive test and revision of the DUV is necessary.

It is common practice for newly adopted codes to be tested, revised and calibrated as implementation begins. This Section provides a series of recommendations to guide the above mentioned code revisions and adjustments:

1. AMEND THE SECTOR PLAN

The Sector Plan needs to be both amended and confirmed. Elements such as the location of a future transit station, centralized parking area/s and ultimate location and configuration of Main Street need to be addressed. More refined transition between zones needs to be established. Areas where retail on the ground floor is mandatory should be indicated in the sector plan/master plan. As these elements are confirmed, requirements for parking, density and intensity should be adjusted. (Section I - P.10 Note10)

2. DETERMINE IF THE DUV FORM BASED CODE IS ORGANIZED AROUND BUILDING TYPES OR LOT TYPES

Form Based Codes are typically structured around building types, street types or lot types. The DUV appears to incorporate and mix all three strategies. The code appears slightly more oriented towards a "Building Type" format, yet fails to clearly identify and codify such types. It also defines building types based on lot configuration (flex-building type). The flex-building type is not a precedent-tested type. Additionally, ownership appears mixed into the types (e.g. the "Stacked Apartment type"). A building that houses multiple residential units is a multifamily residential building. Ownership of units does not pertain to form.

3. DO NOT AWARD ANY BONUSES/INCENTIVES UNTIL THEY ARE DEFINED/CLARIFIED

It is unclear how reserve units are awarded. It appears as if as bonus conditions can be applied in a cumulative manner. Density bonus caps are not defined. Height bonuses do not appear to be sufficiently quantified. There is language in the DUV that states that maximum bonuses are not "a given" (Section I – Note 2). As such, the Village should suspend the award of any type of bonus or incentive until elements are clearly quantified and defined. It is also important to note that the appropriate distribution of residential density should result in the creation of place. Awarding all density reserve units on a first-come, first-serve basis will yield a handful of very dense, scattered buildings and will fail to create a great address/destination.

4. ALLOW ANY BUILDINGS CURRENTLY UNDERGOING REVIEW TO PROCEED AS-OF-RIGHT (WITHOUT ANY BONUSES)

Projects that are currently undergoing review should be allowed to seek permits without any bonuses until an ordinance that defines items stated in item 2 are defined and adopted.

5. AMEND DUV BEFORE APPROVING NEW DEVELOPMENT

The Village can resort to different processing and approval deferrals until certain critical amendments to the DUV are implemented. Density, intensity, transition between sectors and form can and should be quickly addressed. A more granular definition of zones (sector plan) plus an adjustment of parking requirements in certain areas as well as addressing the multiple comments annotated throughout the document (see Section I) need to be in place to ensure future development is consistent with the community's vision for the downtown.

6. REGARDING DENSITY

Densities as proposed (including the reserve units) appear appropriate for the area. However, a more granular distribution of densities, more appropriate transition between sectors, better definition of bonuses (awarding of reserve units and TDR's) and revisions to unit size and types is necessary. The densities proposed should not yield overly massive buildings. Inappropriate parking requirements and allowances, unnecessary design constraints and excessive bonuses appear to be driving mass.

DEFINE THE CENTRALIZED PARKING SYSTEM.

The Centralized Parking can be addressed with a simple ordinance, but with care from the Village not to over-commit to the final outcome. Location for, or at a minimum a strategy to locate a future parking lot or garage needs to be in place. A scaled set of parking improvements need to be defined and more specifically tied to the bonus program. Furthermore, parking should be understood as a "means to achieve a goal" and not as a "goal in itself." It is one of the most powerful tools to define and control mass and scale. Erroneously managed or disproportionately required it leads to unwanted form. Continue to explore this concept and adopt policies that reflect modern parking best practices.

7. CLEARLY DEFINE THE TERM "DESIGN CONSIDERATIONS"

The term "Design Considerations" is mentioned in Section 1.05 (B) beginning of the DUV. Beyond that mention, is not defined. The manner in which it is presented does not entitle the applicant to deviate from any element of the code. The consultant team believes that this term should not be used as a justification to allow buildings to significantly deviate from required form. Clarify the applicability of 'design considerations'. Should they be allowed at all? Which person or body should grant them? (Council, Planning Director, Design Board, etc.)

8. AMEND THE DUV AS FOLLOWS:

- *Re-define and graphically represent the building types or lot types around which the DUV is organized. The DUV appears to define building types differently. Some relate to the lot, others to the use. The intent of some types is not clear: e.g. what is a mixed-use row house? Is it a row-house or a mixed use building of small scale? What is a stacked apartment building? Could this type have condos? Isn't this a multifamily building whose scale can be regulated on a sector-by-sector basis? What is a flex building? Is it a mixed-use building when the mix is required and a multifamily building when it isn't? Flex Building/ Flex Block are not types but rather seem included to allow almost anything. This is in stark contrast to the intent of a building type code, based on time tested building typology.*
- *Clearly state and focus bonuses on achieving specific goals. Focus on providing bonuses to elements that further the intent to achieve a vibrant restaurant/entertainment/retail center and the creation of quality addresses and place. Bonuses should not be awarded for elements that do not benefit the community as a whole.*
- *Prioritize parks and establish a more precise benefit (bonus) as a result of this prioritization.*

- *Amend density inconsistencies for the NV sector.* Table on page 10 states it is 14du/ac and table on page X states it is 24du/ac.
- *The Density plan should coincide with a transect-zone map.* Two densities implies two zones, regardless of heights or building types.
- *Amend the Sector Plan,* an develop a master plan that shows where mixed-use that includes retail/restaurants on the ground floor is mandatory (i.e. main street), where the transit stop is located, where the centralized parking could occur, etc.
- *The DUV identifies a street hierarchy plan – an A/B system of streets.* There is no apparent need for 4 different categories. “A” streets should hold development to the highest standards and “B” streets should clearly relax those. Further define this system and identify different standards for each.
- *Re-address sectors (see Note 19).* The DUV establishes minimum heights to differentiate building mass in different zones, yet allows certain building types to maintain same height and density across all zones. Differences in scale and intensity are defined with maximum heights and changes to building types allowed across zones. As an example, the flexible block is allowed in three out of the four sectors, always at 8 stories with 24 du/ac. How will this generate 3 distinct areas?
- *Address height inconsistencies.* Buildings require a step-back at the 3rd floor throughout every sector except in the NV, where 4 story buildings are allowed without step back. This yields the tallest base/massing in the least intense zone.
- *Expand/redefine private-open space requirements.* Sometimes it is too prescriptive, others too loose.
- *Franjo Road standards need to be well defined.* Consider doing a detailed master plan for this road.
- *Eliminate relaxed rules for parking basements.* Cities that want to control the building-to-street relationships generally do not consider parking levels that are halfway submerged and halfway above ground to be considered “underground.”
- *Underground parking,* which is very expensive, undertaken only to meet high minimum parking requirements, should not be used as a justification for bonuses that lead to excessive building height and out of scale developments. Consider right-sizing minimum parking requirements instead. Basements should not elevate over street level in commercial and mixed-use streets.
- *Quantify requirement for public open space vs. bonus.* It is not clear how much the amount of truly public open space relates to bonus intensity.
- *Consider adding façade transparency and more defined requirements for mid-block passage-ways.*
- *Graphics for specific development parameters should be accurate to the regulations they graphically represent.* These diagrams show more than one reduction in floor plate which is not a code requirement. In the revision of the DUV, additional care should be given to make sure the graphic illustrations are not inadvertently deceptive. Bonus stories should not be represented in the same manner as by-right developed areas.

9. TEST THE CODE

The code should be “tested” to determine what the actual intensity and height yields are at 14 du/ac and 24 du/ac. A second “test” should be conducted with proposed density bonuses at pre-established increases (e.g. 50%, 100%, 150%, etc). The suggested tests should reveal that it is possible to house existing, as-of-right densities within significantly lower as-of-right heights. Until such tests are conducted, height is a bit of a “guessing game.” Consider the following when “testing” the DUV:

- Determine a scaled, maximum as-of-right base height on a sector-by-sector basis (e.g. 2 stories in the NV, 3 in the UV, 4 in the DV and 5 in the DG).
- Consider a scaled bonus story increase. (As an example, bonus stories should be 90% of the base floor plate in the DG, 75% in the UV, 50% in the DV).
- Limit the number of bonus stories by sector (e.g. 1 additional story in the NV and DV, 2 in the DG).
- Consider implementing a true transect, where intensity, density and building type changes are physically evident.

